



# ADA Transition Plan

Town of Wilton, New York



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# **I. Introduction**

## **A. Overview**

The purpose of this ADA Transition Plan is to ensure that the Town of Wilton pedestrian infrastructure in the public right-of-way is accessible for everyone, including people with disabilities. Federal standards for pedestrian facilities in the public right of way proposed in 2011 became effective in September 2023, were adopted by the US Department of Transportation in December 2024, and will be fully enforceable once adopted by the US Department of Justice. The Town has made a long-term commitment to improving the accessibility in the public-right-of-way.

The Town has identified and evaluated pedestrian infrastructure in the public right-of-way including curb ramps, sidewalks, crosswalks, pedestrian signals, pedestrian accessible transit stops, and multi-use trails. This plan is drafted to memorialize the continued progress by the Town to make their pedestrian facilities in the public right-of-way reasonably accessible for all persons.

This document was funded by the Capital Region Transportation Council (Transportation Council), in part through a grant from the Federal Highway Administration, U.S. Department of Transportation. It is limited to pedestrian transportation facilities in the public right of way as well as shared use paths with a transportation function, and does not include any other programs, services, or activities such as buildings, parks, playgrounds, recreational trails, websites, or meeting locations. The views and opinions expressed herein do not necessarily state or reflect those of the U. S. Department of Transportation.

## **B. Mandate – Americans with Disabilities Act**

The federal legislation, known as the Americans with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications.

## **C. Transition Plan**

Title II of the ADA specifically applies to “public entities” (state and local governments) and the programs, services, and activities they deliver. Title II, Article 8, requires that each public agency must develop a Transition Plan (also referred to as “Plan”) to bring any non-compliant programs, services, and activities into compliance.

The Plan shall, at a minimum:

- Identify physical obstacles in a public entity’s facilities that limit the accessibility of its programs, activities, or services to individuals with disabilities;
- Describe in detail the methods that will be utilized to make the facilities accessible;
- Specify the schedule for taking the necessary steps to achieve compliance with Title II and, if the time period of the transition plan is longer than one year, identify steps that will be taken during each year of the transition period; and
- Indicate the official responsible for implementation of the plan.

Transition plans provide a method for a public entity to schedule and implement ADA

required improvements to existing facilities. Before a transition plan can be developed, an inventory of the existing facilities must be developed.

Because ADA is a civil rights law, the federal enforcement entity for the ADA is the Civil Rights Division of the US Department of Justice (DOJ). The DOJ enforces ADA through various means: establishing standards for meeting the requirements of the ADA; coordinating enforcement activities of other federal agencies; and, as necessary, taking legal action to require compliance with ADA by state and local governments.

The United States Access Board is a federal agency that promotes equality for people with disabilities through leadership in accessible design and the development of accessibility guidelines and standards. As a coordinating body, the scope of their recommendations and guidance includes but is not limited to public rights of way, public buildings, and medical facilities.

#### **D. Public Rights of Way Accessibility Guidelines (PROWAG)**

The United State Access Board issued its final rule for accessibility guidelines for pedestrian facilities in public rights-of-way in September 2023. Not yet formally adopted by the US Department of Justice, the Public Rights of Way Accessibility Guidelines (PROWAG) were adopted by the US Department of Transportation in December 2024. They were utilized in evaluating pedestrian facilities in the preparation of this plan.

The PROWAG provides design guidance for a wide variety of facilities in the public right of way, including curb ramps, sidewalks, street crossings, transit stops, and signage. Many of the guidelines are particularly sensitive to people who use a wheelchair or have low vision, though the guidance is intended to eliminate all access barriers for people who have a disability.

#### **E. Public Right of Way (ROW) & Jurisdiction**

Defining jurisdictional boundaries or ownership of facilities within the public ROW and identifying the responsible entity is central to the successful implementation of the ADA Transition Plan. Three potential jurisdictional entities have been identified in preparing this plan including the Town of Wilton, Saratoga County, and the State of New York. There are no County owned or maintained pedestrian facilities within the Town of Wilton.

Although multiple jurisdictional entities possess rights-of-way within the Town of Wilton, the legal responsibility of maintaining and improving non-conforming or deficient facilities does not necessarily fall to the owner of the right-of-way.

According to New York State Highway Law (Chapter 25, Article 7, §140), sidewalks may be installed by the State of New York. However, notwithstanding an agreement to the contrary, once installed, the Town of Wilton is obligated to maintain those sidewalks. Similarly, the Town must maintain sidewalks along county roadways (Chapter 25, Article 7, § 140) unless there is an agreement to the contrary.

The following table (Table 1) identifies the entity responsible for the maintenance and improvement of pedestrian facilities within the Town. This table is representative only of jurisdictional entities at the Town, County, or State level. It is also important to note the policy of the Town according to the Town Code, included in Appendix I of this document for reference, which identifies that property owners are responsible for maintenance, repair, and keeping sidewalks adjacent to or on their property and premises in or along the public street in the Town safe. The Town will work with property owners of inaccessible facilities to address accessibility concerns.

**Table 1: Responsible Entity for Maintenance of Pedestrian Facilities**

<b>Facility Type</b>	<b>Town of Wilton Right of Way</b>	<b>Saratoga County Right of Way*</b>	<b>New York State Right of Way</b>
<b>Sidewalks</b>	Wilton	Wilton	Wilton
<b>Curb Ramps</b>	Wilton	Wilton	Wilton
<b>Crosswalks</b>	Wilton	Saratoga County	New York State
<b>Pedestrian Crossing Signals</b>	Wilton	Saratoga County	New York State
<b>Multi-Use Trails</b>	Wilton	Saratoga County	New York State

*\*There are no County owned or maintained pedestrian facilities within the Town of Wilton.*

## **F. Installation of Conforming Facilities During Roadway Alteration Projects**

In accordance with the Department of Justice(DOJ)/Department of Transportation (DOT) Joint Technical Assistance on the Title II of Americans with Disabilities Act Requirements to Provide Curb Ramps when Streets, Roads, or Highways are Altered through Resurfacing (<https://archive.ada.gov/doj-fhwa-ta.htm>) – when pedestrian walkways intersect a curb, and the adjacent roadway is being altered including by resurfacing, the entity facilitating the roadway alteration is also responsible for the provision of an accessible curb ramp. The same principle exists if an entity is undertaking a roadway alteration project that requires the removal of other pedestrian facilities, such as sidewalks. If the facility is removed, it must be reinstalled and conform to the most current accessibility standards. Guidance on the definition of a roadway alteration is provided in the DOJ document referenced above and include: “reconstruction, rehabilitation, resurfacing, widening, and projects of similar scale and effect.”

## **G. Identified Obstacles in the Public Right-of-Way**

The Town of Wilton has conducted a self-evaluation of obstacles to pedestrian infrastructure along all public rights-of-way within the Town, based on the methodology and criteria outlined in Appendix A. Sidewalks were evaluated for continuity, missing panels, heaving panels, physical obstructions, and minimum width. Curb ramps, crosswalks, and pedestrian signals were evaluated against key PROWAG criteria. The results of the evaluation are presented in the following sections.

## II. Detailed Evaluation

### A. Inventory

An inventory and assessment of all pedestrian facilities located in the Town on local, county, and state roads was completed by staff from the Town of Wilton and consultants provided by the Capital Region Transportation Council (Transportation Council).

Initially, a sidewalk inventory (including related pedestrian features and transit stops) was provided by The Transportation Council and this information was updated based on a review of the most current aerial photography and Google Street View. This information was used as a base map for additional data collection and condition assessment. New sidewalks may have been constructed during or shortly before the completion of this document. Over time, changes to the pedestrian infrastructure should be incorporated into the inventory and monitored for continued compliance.

Field data collection was accomplished on a mobile device (a Tablet, iPad or mobile phone) and the ESRI applications ArcGIS Collector and Survey123. Municipal and consultant staff completed the inventory and collected photographs and other important site-specific information.

Pedestrian features evaluated included sidewalks, multi-purpose paths, transit stops, and key intersection features including curb ramps, pedestrian signals, and crosswalks. Evaluation criteria focused on compliance with key elements adapted from the PROWAG. The evaluation methods are included in Appendix A.

To complete the detailed evaluation, an accessibility condition rating for each of the identified facilities was determined. The Town of Wilton used the following ratings and definitions to assess the condition of each sidewalk, curb ramp, crosswalk, transit stop (when connected to the sidewalk network), and multi-use trail facility on local, county, and state rights-of-way.

**Table 2: Characteristics of Accessibility Condition for Sidewalks and Multi-Use Trails**

	Characteristics	Example
<b>Not Accessible</b>	<ul style="list-style-type: none"> <li>• Significant Discontinuity</li> <li>• Inaccessible</li> </ul>	<ul style="list-style-type: none"> <li>• No Curb Ramps</li> <li>• Steps</li> <li>• Significant Heaving</li> </ul>
<b>Partially Accessible</b>	<ul style="list-style-type: none"> <li>• Limited Accessibility</li> </ul>	<ul style="list-style-type: none"> <li>• Several Minor Defects</li> </ul>
<b>Mostly Accessible</b>	<ul style="list-style-type: none"> <li>• Provide Access</li> <li>• Not Fully Compliant</li> </ul>	<ul style="list-style-type: none"> <li>• Curb Ramps without Detectable Warnings</li> <li>• Limited Minor Defects</li> </ul>
<b>Fully Accessible</b>	<ul style="list-style-type: none"> <li>• Designed to Meet Current Standards</li> </ul>	<ul style="list-style-type: none"> <li>• Newly Installed Facility</li> </ul>

*Note: Additional information on evaluation criteria, including example imagery of sidewalk and trail conditions, can be found in Appendix A.*

To evaluate the accessibility of more complex multi-part features like curb ramps, crosswalks, and transit stops the Town developed a rating matrix. Depending on the number of deficiencies at any one of these features, they were assigned a pass/fail rating.

The inventory of all pedestrian features is presented in Appendix B. The results of the evaluation are presented below and are also depicted in Appendix B.

## B. Sidewalks

The Town of Wilton has assessed a total of 2.7 miles of sidewalk for compliance with PROWAG. Sidewalks are assessed for missing panels, heaving panels, obstructions and panel sidewalk width. Approximately 0.15 miles of sidewalk are classified as “Not Accessible” Some of these sidewalks are expected to be replaced by a pedestrian/bike path in 2026 Also, 0.1 miles are classified as “Partially Accessible”. Most of the segments are classified as fully accessible (56.5 percent) or mostly accessible (37.1 percent).

**Table 3: Sidewalk Score Results**

Accessibility Rating	Percentage of Total Miles	Length (Miles)	Number of Segments	Percentage of Segments
<b>Fully Accessible</b>	55.3%	1.5	35	56.5%
<b>Mostly Accessible</b>	36.1%	1.0	23	37.1%
<b>Partially Accessible</b>	3.0%	0.10	2	3.2%
<b>Not Accessible</b>	5.7%	0.15	2	3.2
<b>Grand Total</b>	100.0%	2.7	62	100

A detailed table showing the Town’s inventory of sidewalks is included in Appendix C.

**Table 4: Block Level Sidewalk Barrier Characteristics**

Barrier Characteristic	Number of Blocks With Barrier	Percent (%) of all Blocks
<b>Obstruction</b>	9	7.2%
<b>Too Narrow</b>	4	3.2%

## C. Curb Ramps

All intersections where sidewalks intersected roadways were observed for the presence of curb ramps and whether the curb ramp was flush with the roadway and whether a detectable warning strip (DWS) was present. 56 points were collected to evaluate the condition and accessibility of areas.

Most curb ramps appear to be new, are fully accessible, and are located along State roads. The primary deficiency is the absence of detectable warning strips (DWS) which are used to indicate to the visually impaired that they have reached an intersection with a roadway.

**Table 5: Curb Ramp Conditions**

Type of Deficiency	Count	Percentage of Intersection Points (56 Points)
No Curb Ramp Present	11	19.6%
Curb Ramps Without Barriers	12	21.4%
Curb Ramps With Barriers	33	59.0%
DWS with Barriers*	19	33.9%

\*Curb ramps and DWS surfaces may have multiple barriers

**Table 6: Curb Ramp Barriers**

Type of Deficiency	Count	Percentage of those with Curb Ramps (45 Curb Ramps)
Not Flush with Asphalt	1	2.3%
Inadequate Width	3	6.7%
DWS Absent	29	64.4%
DWS with Barriers*	19	42.2%

\*Curb ramps and DWS surfaces may have multiple barriers

**Table 7: DWS Barriers**

Type of Deficiency	Count	Percentage of those with DWS (27 DWS)
Insufficient Width	0	0.0%
Insufficient Depth	0	0.0%
Insufficient Contrast	3	11.1%
Deterioration/Damage*	19	70.4%

\* DWS surfaces may have multiple barriers

**Table 8: Curb Ramp Inventory Characteristics**

Curb Ramp Characteristic	Count	Percentage of Curb Ramps With DWS
Ramp Serves One Direction of Travel	53	94.6%
Ramp Serves Two Directions of Travel	3	5.4%

*Table 9: DWS Inventory Characteristics*

DWS Inventory Characteristics	Count	Percentage of DWS
DWS Truncated Domes	27	100%
DWS Pavement Grooves	0	0.0%

#### D. Pedestrian Signals

Pedestrian signals let motorists know that pedestrians have the right of way, and let pedestrians know when it is their turn to cross a roadway. Approximately 65% of the locations did not have pedestrian signals. It is important to note that although 16 pedestrian signals were identified as visual only signals, all of these facilities fall under State Jurisdiction to be addressed.

*Table 10: Pedestrian Signal Deficiencies*

	Count	Percentage
No Push Button	0	0
Visual (flashing) Only	16	100% (of signals with a push button)
Audible Only	0	0.0%
Both	0	0.0%
Not Wheelchair Accessible	0	0.0%

#### E. Pedestrian Crossings

*Table 11: Pedestrian Crossing Conditions*

	Count	Percentage
Incomplete Access Route	0	0.0%
Fair or Poor Route Condition	0	0.0%
Cracks/Potholes	0	0.0%
Improper Cross Slope	0	0.0%
Other	0	0.0%

**Table 12: Crosswalk Marking Conditions**

<b>Condition</b>	<b>Count</b>	<b>Percentage</b>
Fair or Poor Crosswalk Condition	2	13.3%

## **F. Transit Stops**

The Capital District Transportation Authority (CDTA) provides transit service to the Town. Two transit stops were assessed for accessibility. Transit stops within 50 ft of sidewalks were assessed based on their connectivity to existing sidewalks and the conditions of their boarding and alighting area. A boarding and alighting area is defined in PROWAG as a level and stable surface for boarding vehicles. One of the two stops had an accessible boarding and alighting area. One of the transit stops was not connected to the sidewalk network.

**Table 13: Transit Stop Conditions**

<b>Transit Stop Condition</b>	<b>Count</b>	<b>Percentage of all Stops</b>
No barriers	1	50%
Not Connected to Sidewalk Network	1	50%
Boarding and alighting areas with barrier *	1	50%
Obstructed Access Route	1	50%
Insufficient Width	0	0%
Insufficient Depth	0	0%
Non-Accessible Surface Material**	0	0%

\*Note: Boarding and alighting areas may have multiple barriers identified

\*\*Note: Grass or dirt surface

Figure 1: Sidewalk Assessment

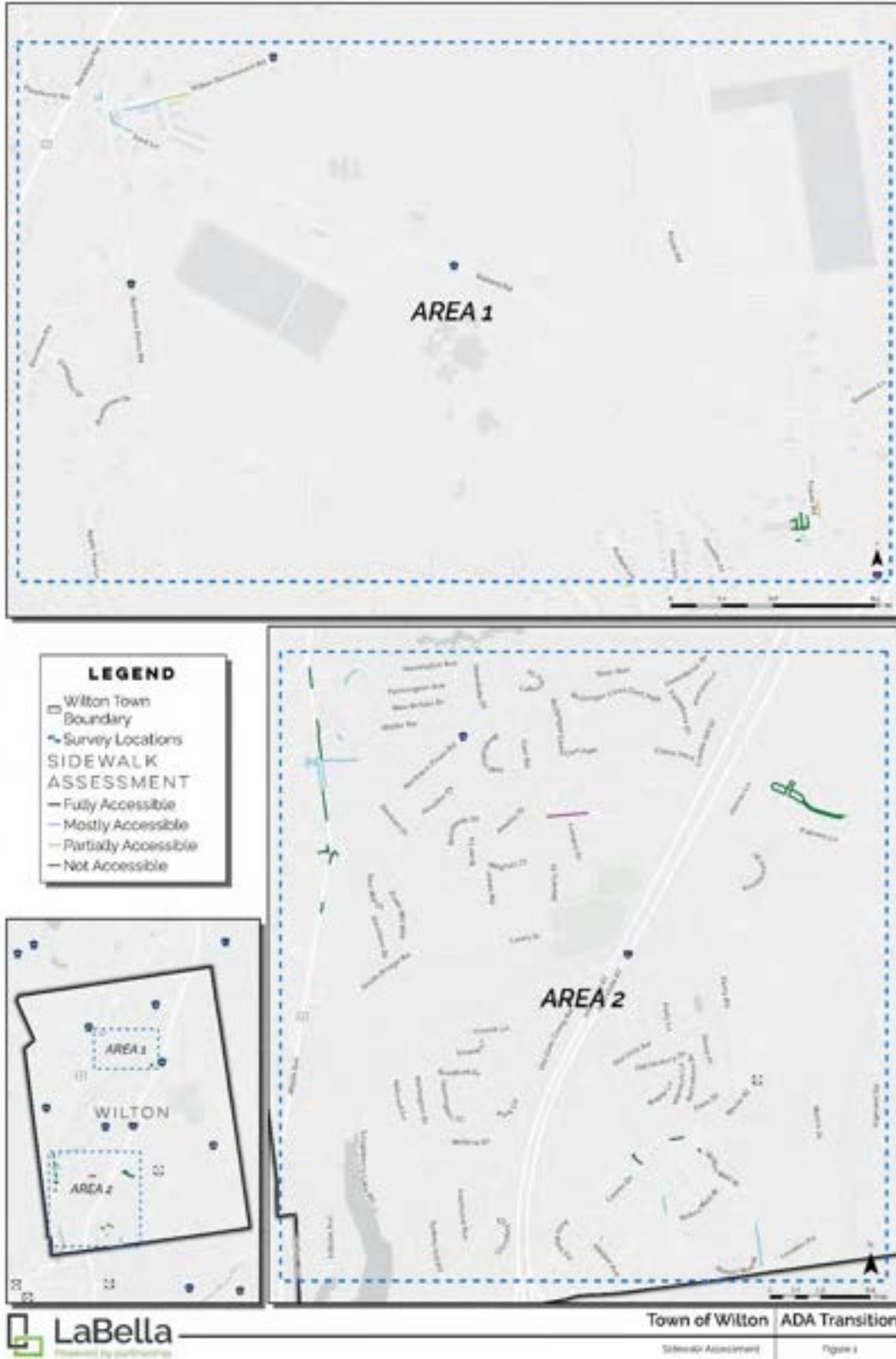


Figure 2: Intersection Assessment



**LEGEND**

- Wilton Town Boundary
- ⋯ Survey Locations

**CURB RAMP ASSESSMENT**

- Fully Accessible
- Mostly Accessible
- Partially Accessible
- Not Accessible



Figure 3: Crossing Assessment

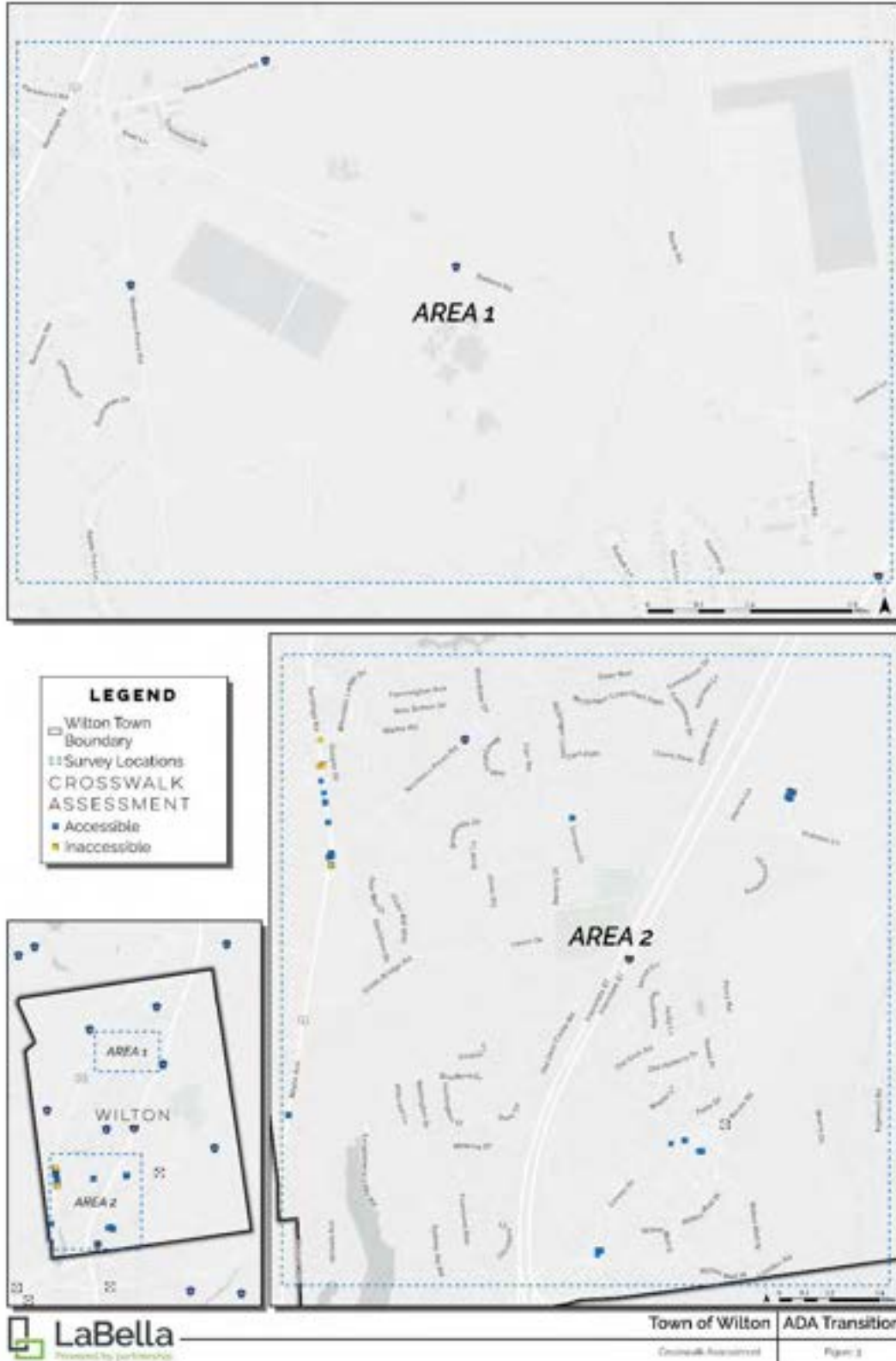
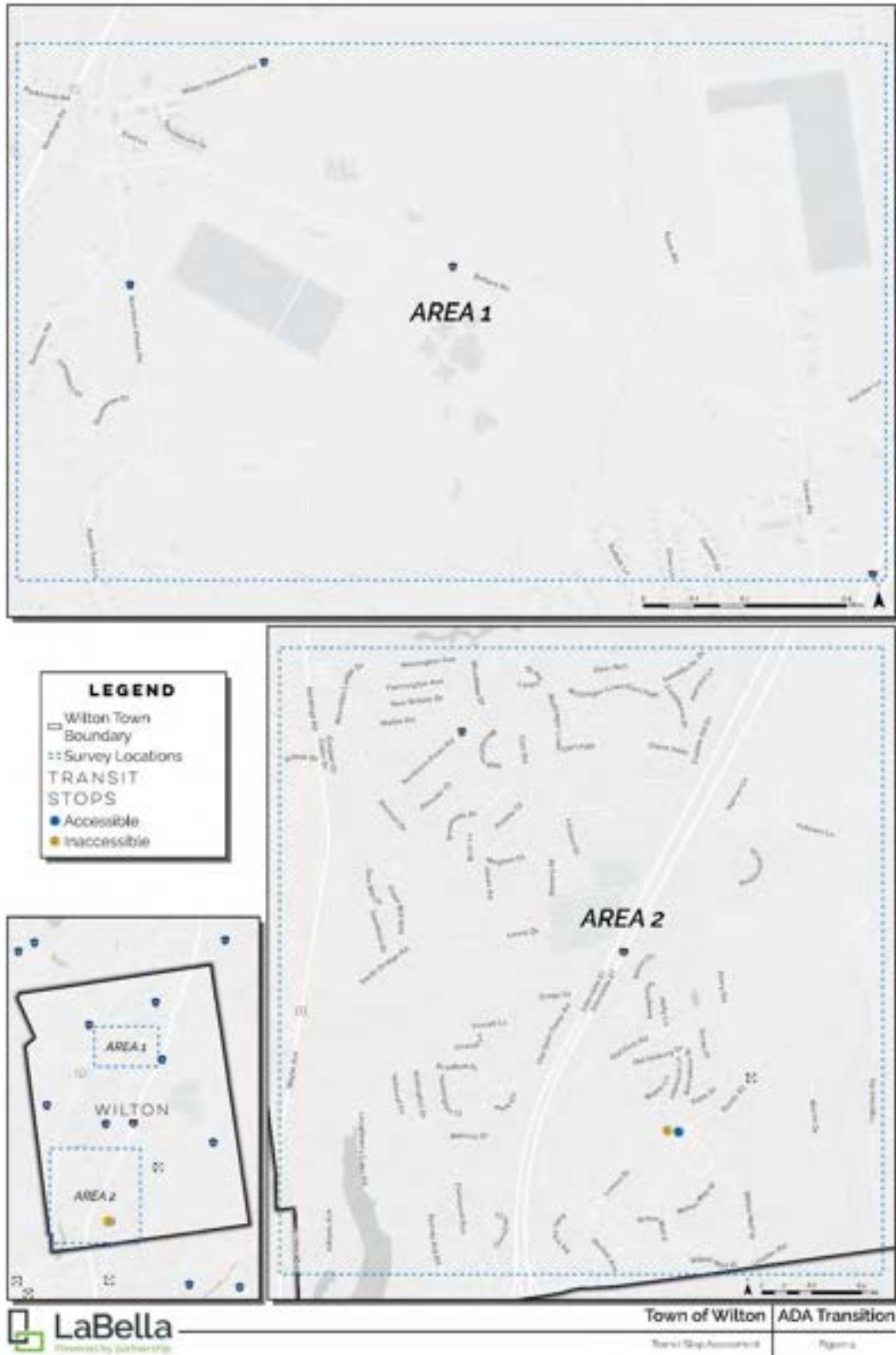


Figure 4: Transit Stop Assessment



### **III. Methods to Remove Barriers – Policies & Priorities**

The Town of Wilton plans to utilize several approaches to removing barriers in the public right-of-way, including proactively identifying and eliminating the barrier, working with property owners to repair their sidewalks, responding to public complaints and ensuring the appropriate design and build-out of new construction following the most recent design guidelines.

The Town of Wilton bases barrier removal priorities on two factors: location and the accessibility condition of the facility.

#### **A. Location Priority**

According to the ADA Title II regulations, the transition plan shall give “priority to walkways serving entities covered by the Act, including State and local government offices and facilities, transportation, places of public accommodation, and employers, followed by walkways serving other areas” (§ 35.150 Existing Facilities (d)(2)).

Following this guidance, the Town of Wilton identified its location priority as follows, beginning with the highest location priority:

**Priority 1:** Facilities serving government facilities, parks and recreation areas.

**Priority 2:** Facilities serving hamlet zones;

**Priority 3:** Facilities serving dense retail centers;

**Priority 4:** Facilities serving residential areas; and

**Priority 5:** Facilities serving other areas

Figures 5 and 6 illustrate the priority sidewalk segments regardless of jurisdiction. Full-size figures are presented in Appendix B.

Figure 5 reflects the prioritization method only for facilities identified through data analysis as “Not Accessible” or “Partially Inaccessible.” Please note, that there are only four segments that were assessed as not or partially accessible, those are the only segments labeled on the map. Alternatively, Figure 6 identifies all existing sidewalk facilities by the prioritization method, regardless of their current accessibility.

Figure 5: Prioritization Map by Existing Condition

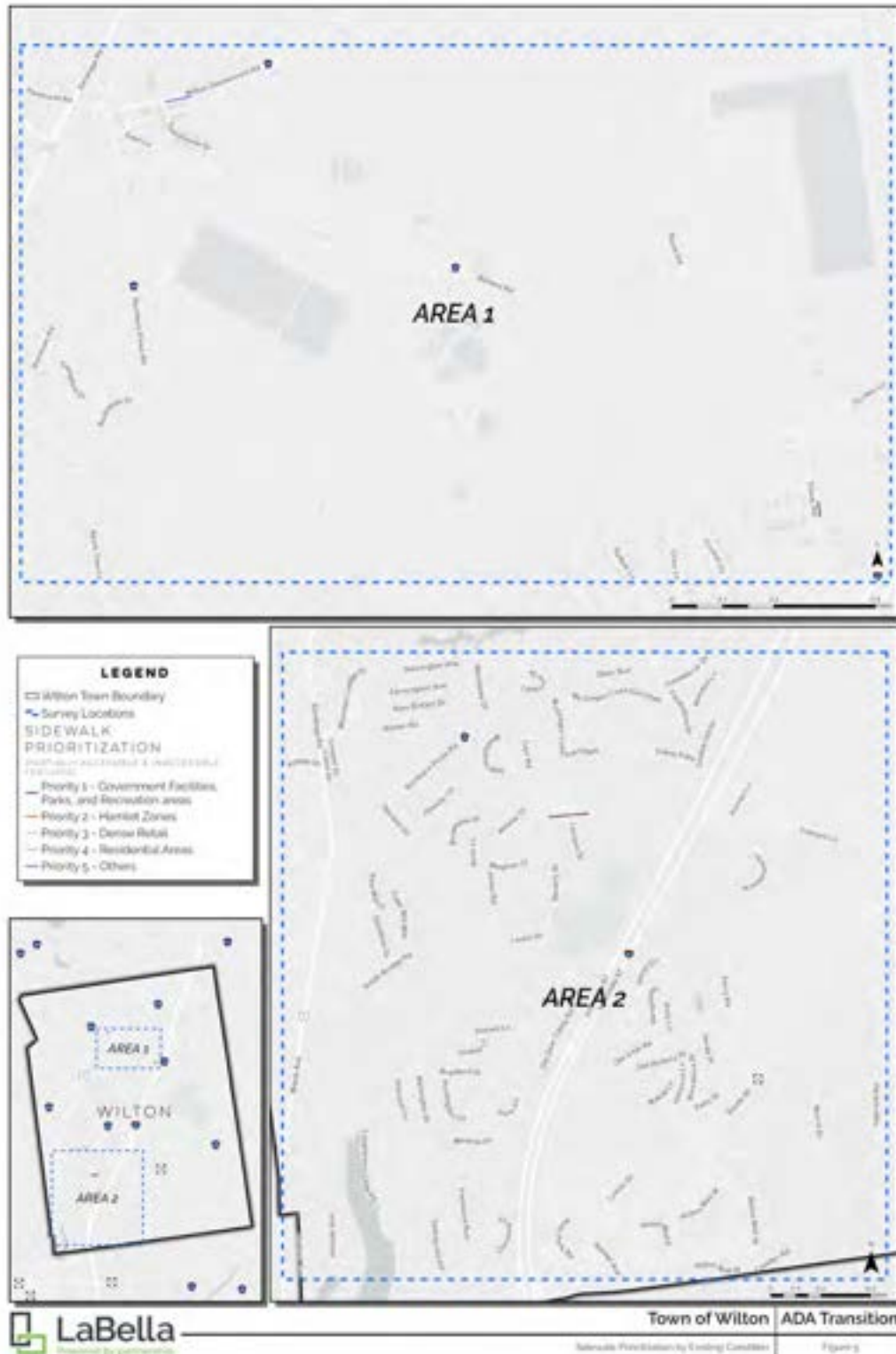
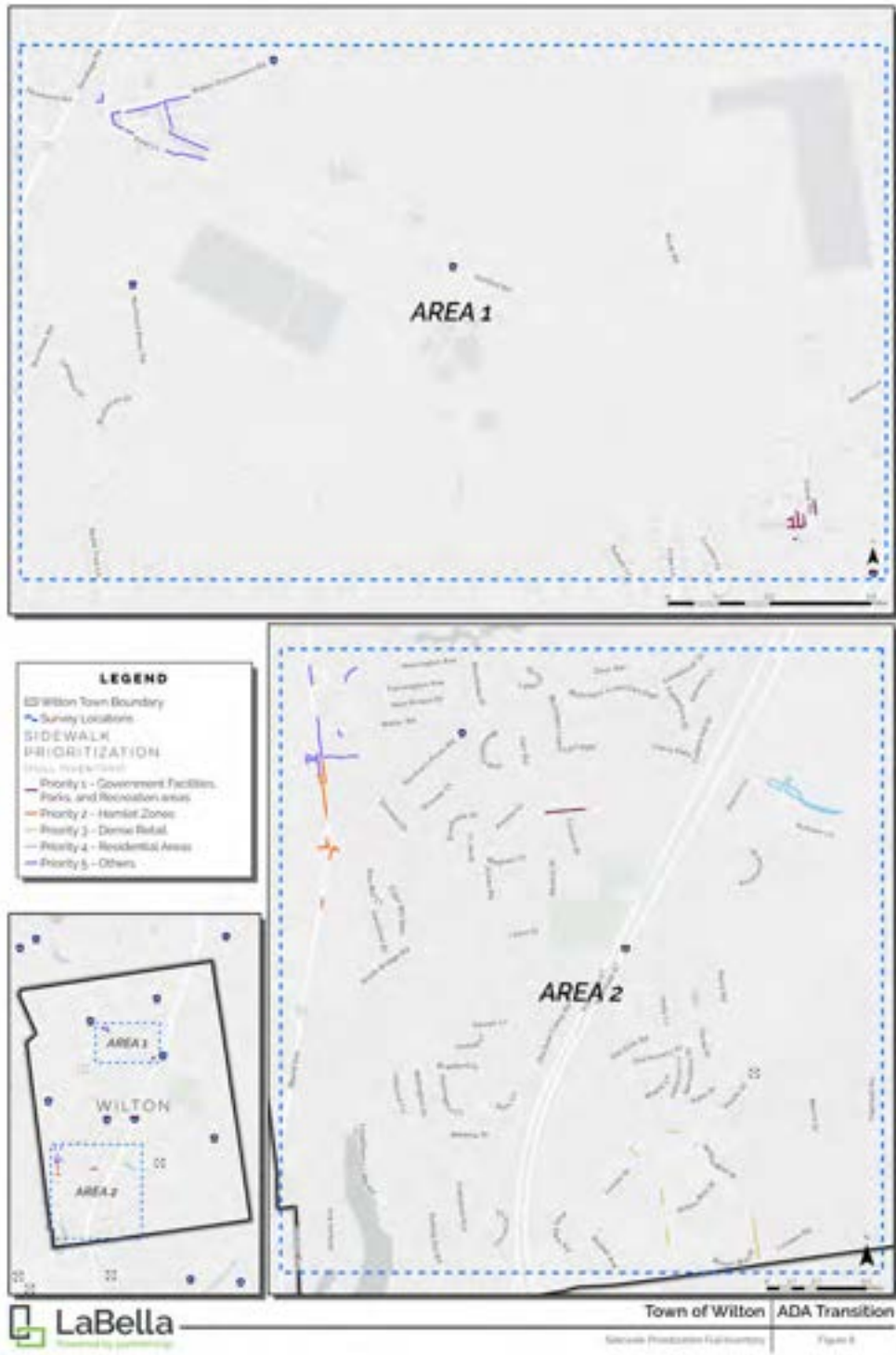


Figure 6: General Prioritization Map



## B. Public Complaint Process

The public complaint process is an integral part of this Transition Plan. Public complaints or requests may often drive the prioritization of improvements. Complaints could cause a re-evaluation of the prioritization to consider changes over time. Those wishing to file a complaint or a request regarding accessibility of a sidewalk or curb ramp should contact the ADA Coordinator, in writing, electronically to [info@townofwilton.com](mailto:info@townofwilton.com) or by another alternative means to accommodate those persons with disabilities.

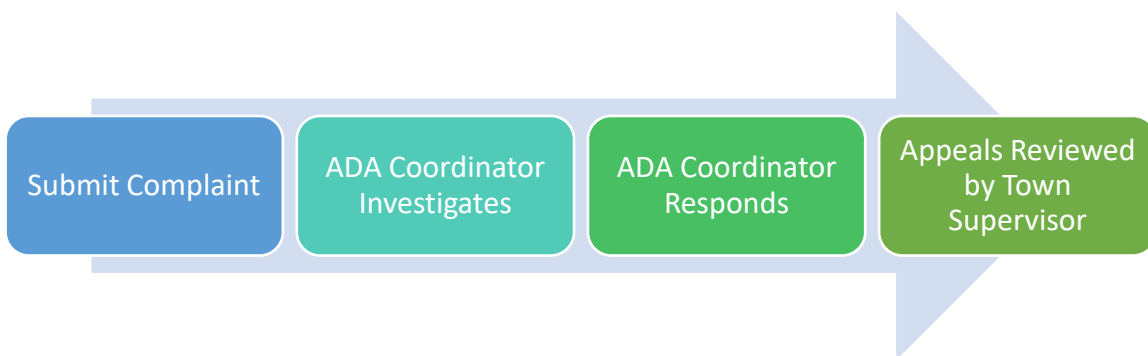
The complaint should be submitted no later than 60 calendar days from the date of the alleged violation. Within 15 calendar days the ADA Coordinator will meet or contact the complainant to discuss possible resolutions. Within 15 calendar days following this meeting or contact with the ADA Coordinator, the ADA Coordinator will respond in writing and/or by an alternative means to accommodate the complainant. This response will explain the position of the Town of Wilton and offer substantive resolutions to the complaint.

If within 15 calendar days, the complainant is not satisfied with the determination and resolution by the Town, the complainant may file an appeal with the Town Supervisor or their designee. Within 15 calendar days from the receipt of the appeal, the Town Supervisor will meet or contact the complainant to discuss the complaint and possible resolutions. Within 15 calendar days from the meeting or contact the Supervisor will respond in writing to the complainant with a final resolution on the complaint.

All documents related to the complaints must be retained by the Town for a period of at least three years.

For more information on the Town of Wilton's adopted Grievance Procedure under the Americans with Disabilities Act and the name and contact information for the Town of Wilton ADA Coordinator, visit the Town website at [www.townofwilton.com](http://www.townofwilton.com). This information can also be found in Appendix E and Appendix F, respectively.

**Appendix F** is a copy of the Town's public Grievance Procedure under the Americans with Disabilities Act.



## C. New Construction & Alterations

In order to ensure the correct design of curb ramps, sidewalks, and crosswalks in new construction and alteration, the Town, through the adoption of this Transition Plan, has adopted the Public Rights of Way Accessibility Guidelines (PROWAG) as finalized in 2023, (herein after known as ADA Guidelines) or the most current regulatory guidelines in the event PROWAG becomes outdated. Once adopted by the US Department of Justice,

PROWAG will be the primary regulatory standards that govern the design and construction of all pedestrian facilities. It is important to note that in alteration projects, whenever pedestrian facilities cannot fully meet the standards outlined in the ADA Guidelines, a determination of each structural implacability or technical infeasibility must be thoroughly documented in the permanent project record.

The Town of Wilton's commitment to improving the accessibility of public facilities is clearly demonstrated by the Work Plan (Appendix D) and schedule outlined in this report.

The Town of Wilton is responsible for the improvement of the facilities identified in the work plan found in Appendix D of this plan. However, as previously discussed under the Jurisdiction section of this report, if another jurisdiction is undertaking alterations of a roadway, including repaving, that agency is responsible for the replacement of removed pedestrian facilities or for the construction or rehabilitation of a curb ramp to the adjacent sidewalk if an accessible curb ramp does not exist. Also, as noted above, the Town of Wilton will work with property owners responsible for maintaining the safety of sidewalks adjacent to or on their property and premises in or along the public street in the Town to address inaccessible facilities.

As described in NYSDOT's current Transition Plan, NYSDOT undertakes on-going road maintenance and alterations to ensure accessibility of pedestrian facilities on State owned roads. For the purposes of this Transition Plan, areas lacking the provision of curb ramps in State owned rights-of-way will be identified in this plan as being undertaken by NYSDOT. The improvement of these facilities will correspond to the approved NYSDOT capital improvement program. Nothing contained in this work plan would in any way prevent the Town of Wilton from modifying the implementation schedule.

#### **D. Cost and Schedule**

The detailed evaluation of the Town of Wilton's sidewalk network identified a total of 0.3 miles of sidewalk segments rated Partially Accessible or Not Accessible and 14 curb ramps, and 48 DWS, and 16 pedestrian signals which will require repair or replacement of barriers to bring them to compliance. For the purpose of estimating construction costs, it was assumed that 30 percent of Not Accessible sidewalk segment panels will require replacement and 15 percent of the Partially Accessible segment panels will require replacement. All 16 pedestrian signals fell along State Routes and are therefore the jurisdictional responsibility of NYS.

The cost of repairs and replacement was estimated utilizing the NYSDOT Quick Estimator Reference. This estimate is provided as Appendix C and is summarized as follows: Utilizing the scoring of facilities provided, sidewalk replacement totals \$23,760, Curb Ramps \$88,200.00, and DWS \$96,000. It should be noted that the actual costs will vary depending on the extent of repairs, actual site conditions, and the date of construction. These cost estimates have also not been adjusted for inflation.

The Town of Wilton intends to fix inaccessible sidewalk segments through a sidewalk replacement project in 2026 and will bring all facilities into compliance in approximately 3 years. Sidewalks and curb ramps are intended for the first year, followed by DWS in the second year, and any remaining facility improvements in the third, contingent on the availability of funding. Curb ramps, and crossings may also be addressed with their corresponding sidewalk segments or during road paving and resurfacing projects. As identified during the location prioritization process, pedestrian facilities in the public right of way adjacent to government facilities and schools, commercial and employment cents, and residential areas will be prioritized before facilities serving other areas.

As opportunity allows, the Town will make efforts to collaborate with stakeholders to improve

the ADA accessibility of pedestrian facilities in the public right-of-way. Compliance is required to the extent feasible within the scope of the project. There will be times when it is technically infeasible to provide full compliance with the guidelines for example, if clear space at the top of the ramp is obstructed by a building or the slope of a hill is so extreme as to prevent a reasonable slope for a ramp in both directions. The inventory process may not account for such situations and could show a high-priority rating when all practical actions have been taken.

## **IV. Monitoring and Updates**

### **A. Transition Plan Management**

As with any effective planning document, the Town of Wilton's Transition Plan will require continued review and updating. It is recommended that the Town review and update this document on a five (5) year cycle. However, this does not preclude the Town from altering that five-year cycle. If new regulations are adopted or new information is available that does not alter the intent of the transition plan, the Town intends to add appendices to the document reflecting those changes. As a growing Town, Wilton will need to reevaluate where the second and third priority areas are as new development occurs. New development may result in a concentration of activity centers and residential units and thereby make one area a higher priority for improvements.

Members of the public that wish to see changes made to the plan are encouraged to submit their comments and concerns, in writing, to the ADA Coordinator to consider including during the next update.

In addition, the Town will document progress made annually, for public review and comment. Members of the public that wish to see changes made to the plan are encouraged to submit their comments and concerns, in writing, to the ADA Coordinator to consider including during the next update.

### **B. Coordination with Public Improvements and Capital Expansion**

The Town has identified a schedule for improvement to non-compliant facilities (Appendix D). However, this schedule may be modified to respond to complaints and to take advantage of regular capital improvement projects or private development to upgrade deficient facilities, if occurring adjacent to those facilities.

The Town will coordinate with the New York State Department of Transportation (NYSDOT) capital improvement schedule to replace or upgrade non-compliant curb ramps and sidewalks along State highways. Likewise, the Town will work with the County to replace or upgrade non-compliant curb ramps and sidewalks along County roads.

### **Public Input**

The Town of Wilton provided opportunities for individuals to comment on this Transition Plan, which includes:

- Copies of the ADA Transition Plan will be made available at the Wilton Town Hall, 22 Traver Road, Wilton, NY 12831
- The ADA Transition Plan will also be made available on the Town's website at

[www.townofwilton.com](http://www.townofwilton.com); and

- The Town Board will discuss and open a 30-day review period beginning on April 3rd, 2024 before the adoption of the ADA Transition Plan. Written comments should be sent to the ADA Coordinator to the address listed in Appendix G or electronically to [info@townofwilton.com](mailto:info@townofwilton.com).

## **Public Comment**

Comments during this period included the following:

- **Comments received will be listed here**

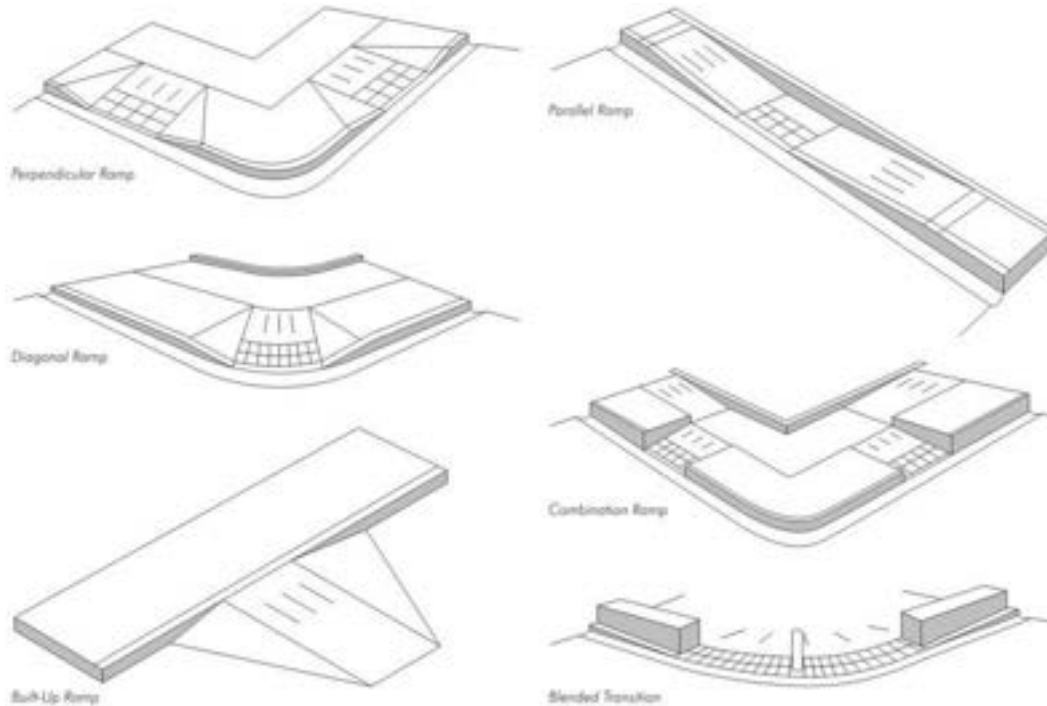
**Appendix A: Evaluation Criteria**

## PART A: Intersections

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### 1. Curb Ramps:

*There are many different types of curb ramps that can be used for different intersection types.*



### 2. Detectable Warning Surface (DWS) Type:

**Truncated Domes:** Bubble like domes appear popped out of the DWS. They should have a height of around 0.2 inches. The domes should be aligned in the direction of pedestrian travel.

**Pavement Grooves:** Grooves cut in the existing concrete, not an ideal DWS. The grooves should be aligned in the direction of travel.



*Truncated domes*

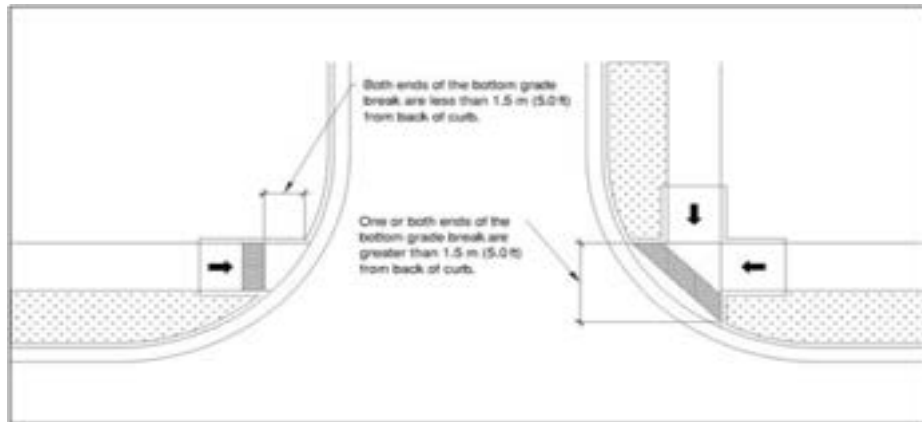


*Pavement grooves*

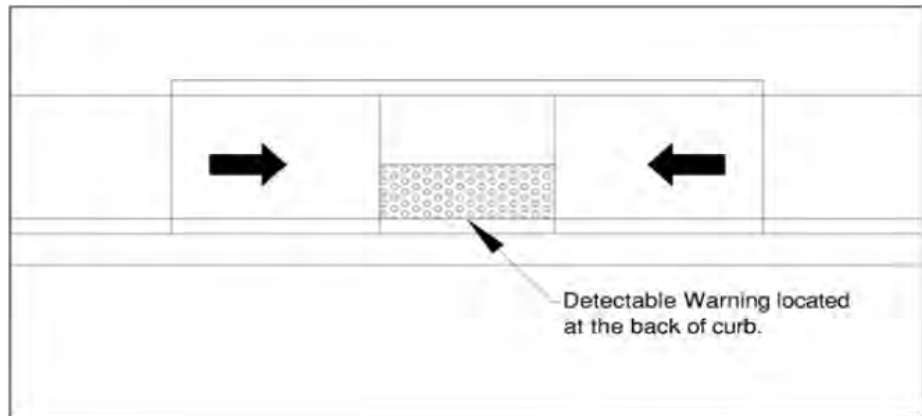
### 3. Detectable Warning Surface (DWS) Placement, Width, and Depth:

**Placement:** The DWS should be aligned perpendicular to the pedestrian direction of travel. This depends on the type of curb ramp. Follow the figures below for the appropriate curb ramp.

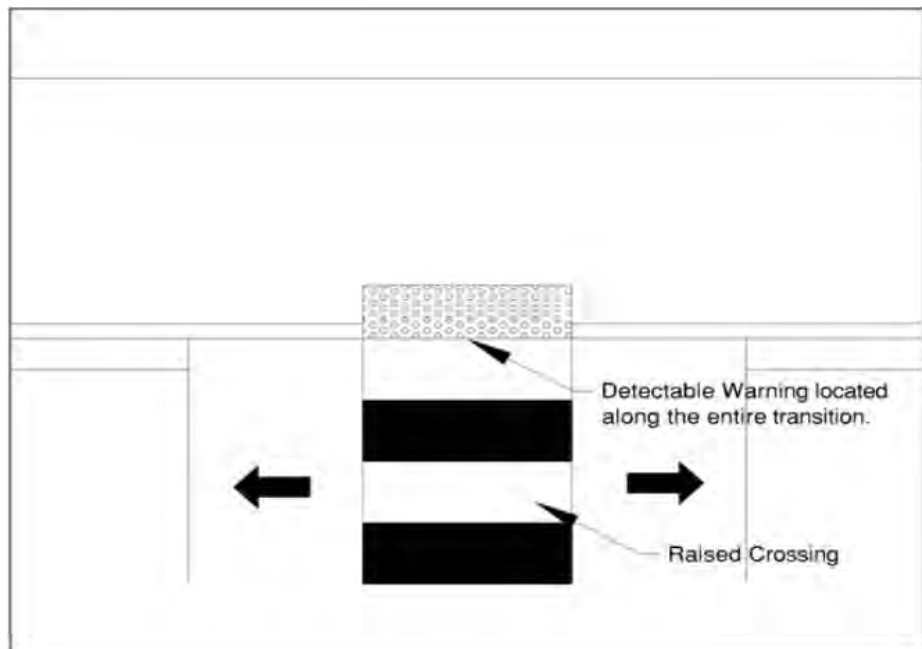
*Perpendicular curb ramp*



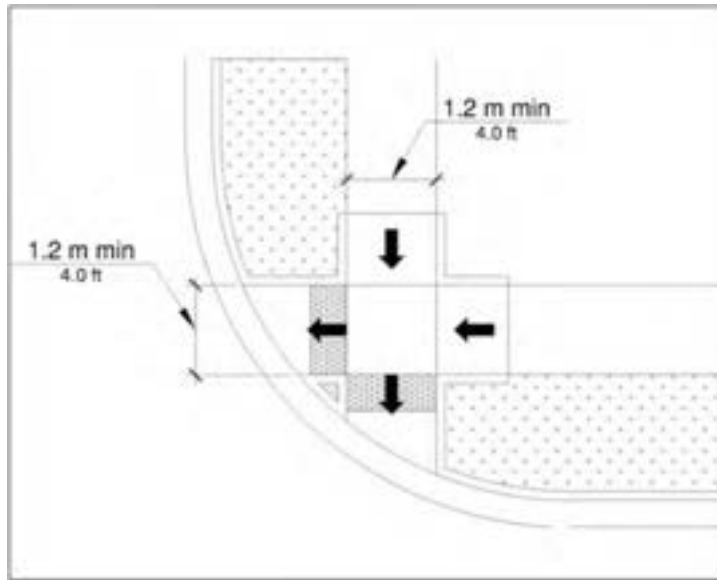
*Parallel curb ramp*



*Blended transition*

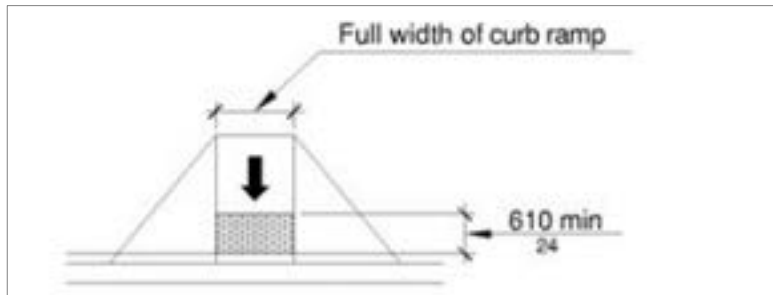


**Width:** The DWS should extend the full width of the curb ramp or landing space. The minimum is 4 feet (48 inches).

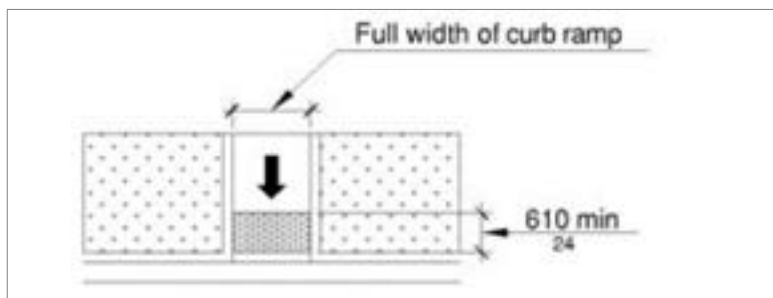


**Depth:** The DWS should be a minimum of 2 feet (24 inches) in depth in the direction of travel.

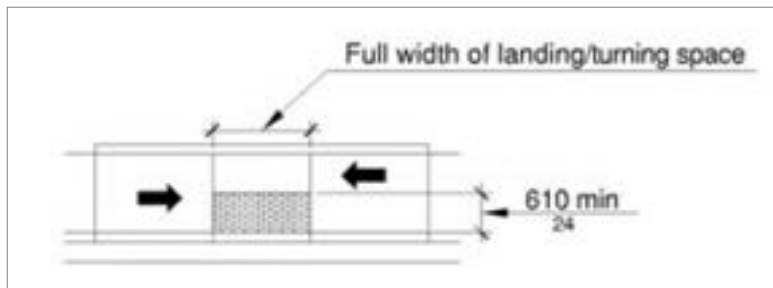
*Perpendicular curb ramp with flared edges*




*Perpendicular curb ramp with returned edges*



*Parallel Curb ramp*



4. Contrasting Detectable Warning Surface (DWS) Color:

<b>DWS Color Contrasts from Curb Ramp</b>	<b>DWS Color Does <u>NOT</u> Contrast from Curb Ramp</b>
 <p>The first photograph shows a red, textured mat placed on a concrete curb ramp, providing a high-contrast visual cue. The second photograph shows a yellow, textured mat placed on a concrete curb ramp, also providing a high-contrast visual cue.</p>	 <p>The first photograph shows a light-colored, textured mat placed on a concrete curb ramp, which does not provide a high-contrast visual cue. The second photograph shows a red, textured mat placed on a brick curb ramp, which does not provide a high-contrast visual cue.</p>

## 5. Type of Pedestrian Signal:

**Pedestrian Sign – Not pedestrian activated:** A sign marked for pedestrian crossing is used. No button mechanism is used. It may or may not have a flashing feature on it.



**Pedestrian Signal – Pedestrian activated:** A button can be pushed by the pedestrian, it then activates a crossing. It may or may not have a flashing feature on it.



### Features of the pedestrian crossing buttons:

**Visual:** High contrast colors are used on the sign and/or button to aid the visually impaired. Flashing lights, large/bold fonts and arrows may also be included.

**Audible:** Locating tones would be emitted from the signal post to guide someone to the pedestrian crossing button. Or when pressed the signal will speak, communicating to the pedestrian when it is safe to cross.

**Both:** When a button uses both auditory signals as well as visual cues to aid a pedestrian in crossing the street. This includes any combination of visually high contrast signage, flashing lights/signals, or audible tones/speech.

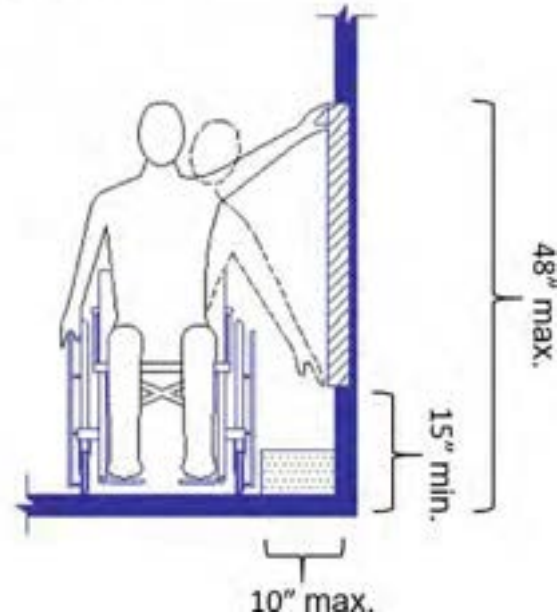
## 6. Pedestrian Signal Compliance:

If the pedestrian signal has a button, the button should be mounted within 15 inches to 48 inches above the ground. The button should be accessible with a maximum of a 10 inches side reach.

### Accessible Pedestrian Pushbuttons

#### Reach Ranges

- 48" max. above the ground
- 15" min. above the ground
- Side reach within 10"







## 7. Crosswalks:

In evaluating the accessibility of existing crosswalks, the following factors are to be considered:

1. Is there a crosswalk that connects two sidewalks across the street?
2. Is the width of the marked crosswalk at least 6 feet?
3. Does the cross slope of the crosswalk meet the following guidelines:
  - a. If the crosswalk is crossing a street with a stop control, is the cross slope less than 2%?
  - b. If the crosswalk is crossing a street without stop control, is the cross slope less than 5%?
4. Is the running slope of the crosswalk less than 5%?
5. If the crosswalk crosses a median, is the length of the median at least 6 feet and does it contain detectable warnings located at a curb line or edge of the roadway?
6. If the intersection is signalized, does it have a pedestrian signal, if so, does the pedestrian sign phase allow enough time for a walking speed of at least 3.5 ft/sec?

PART B: Sidewalks

1. Sidewalk Segment Rating:

Rating	Condition	Reference Images
<p><b>Fully Accessible</b></p>	<p>Sidewalk is new or in like-new condition. Sidewalk is level, no cracks, no faults, no obstructions, and adequate width. Sidewalk is at least 4 feet (48 inches) wide</p>	
<p><b>Mostly Accessible</b></p>	<p>Sidewalk is almost in a new condition. Sidewalk should be level, and adequate width. Sidewalks may have minor cracks</p>	
<p><b>Partially Accessible</b></p>	<p>Sidewalk may have small vertical faults larger than 1/2" but could potentially be beveled/repared, large cracks, an inadequate width, small patches of spalling or another type of rough surface deterioration, and a small buildup of dirt/debris, creeping of vegetation between panels or cracks.</p>	
<p><b>Not Accessible</b></p>	<p>Sidewalk may be seriously uneven from faulting (greater than 1/2 inch) that cannot be beveled/repared and will require replacement, and inadequate width. Sidewalk may have serious accumulation of dirt/debris, establishment of vegetation or other obstructions.</p>	

2. Sidewalk Defects:

*Missing Panels*



*Heaving Panels*



*Obstructions*



*Too Narrow*

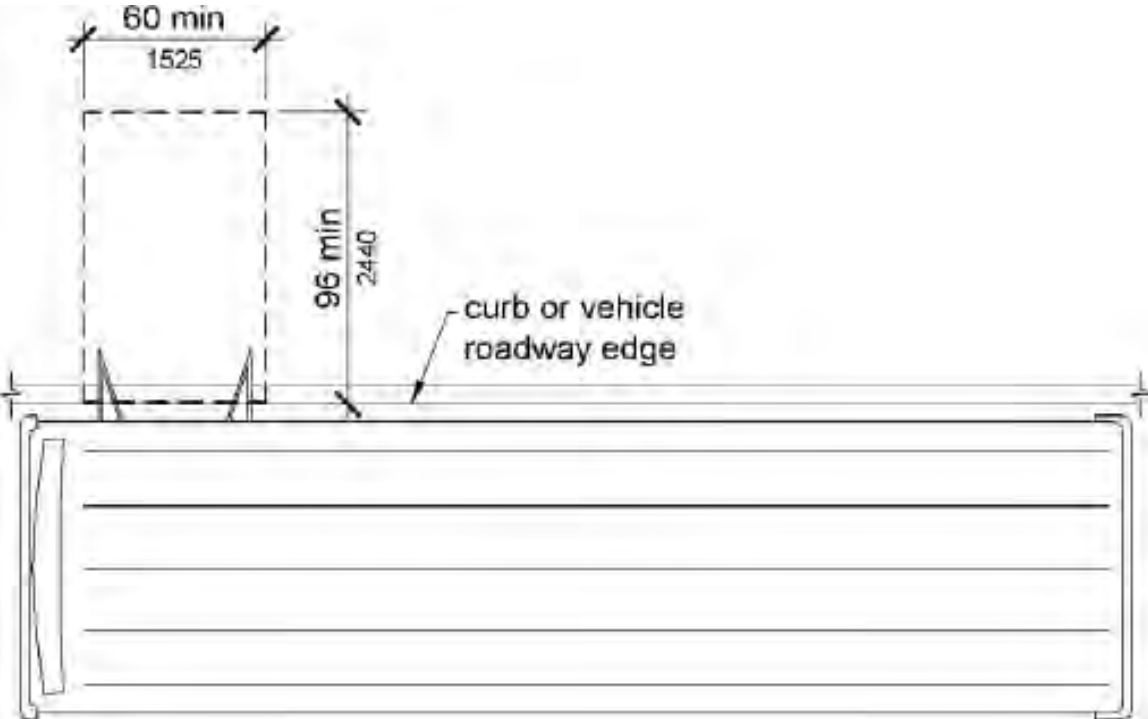


PART C: Transit Stops

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1. Accessible Transit Stops:

Accessible transit stops require a 5 feet (60 inches) wide landing area and an 8 feet (96 inches) deep landing zone.



## **Appendix B: Mapping**

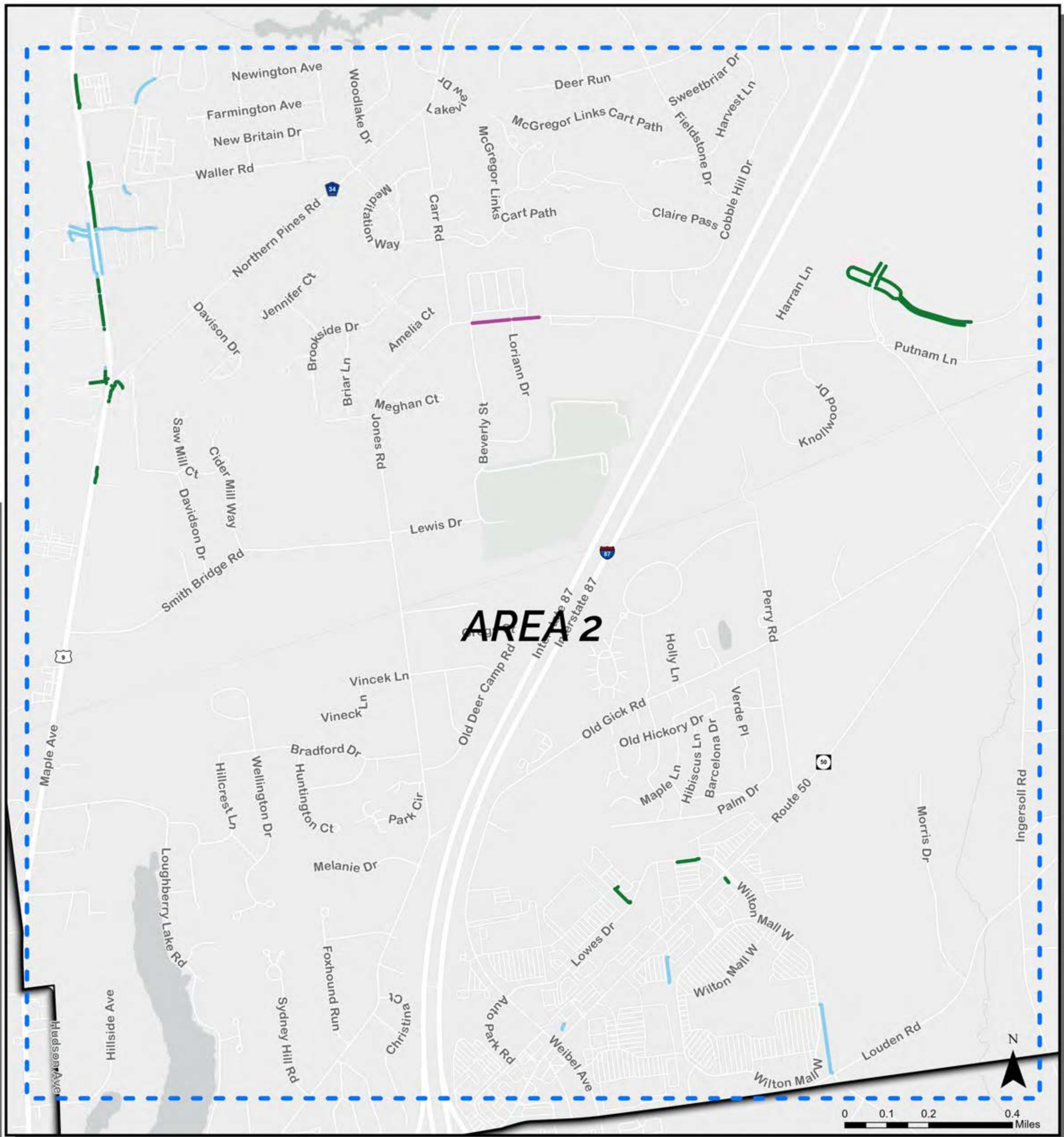
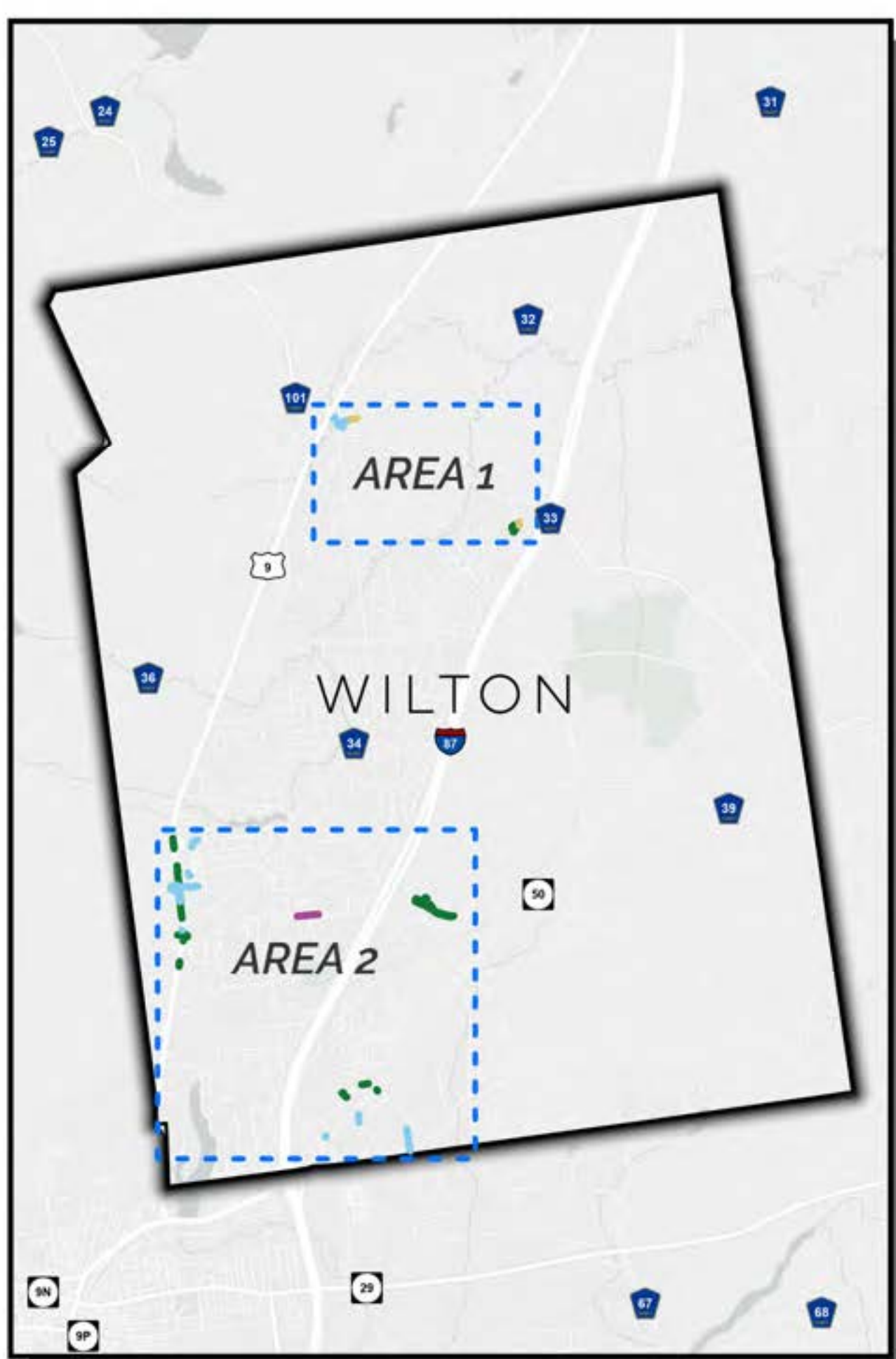


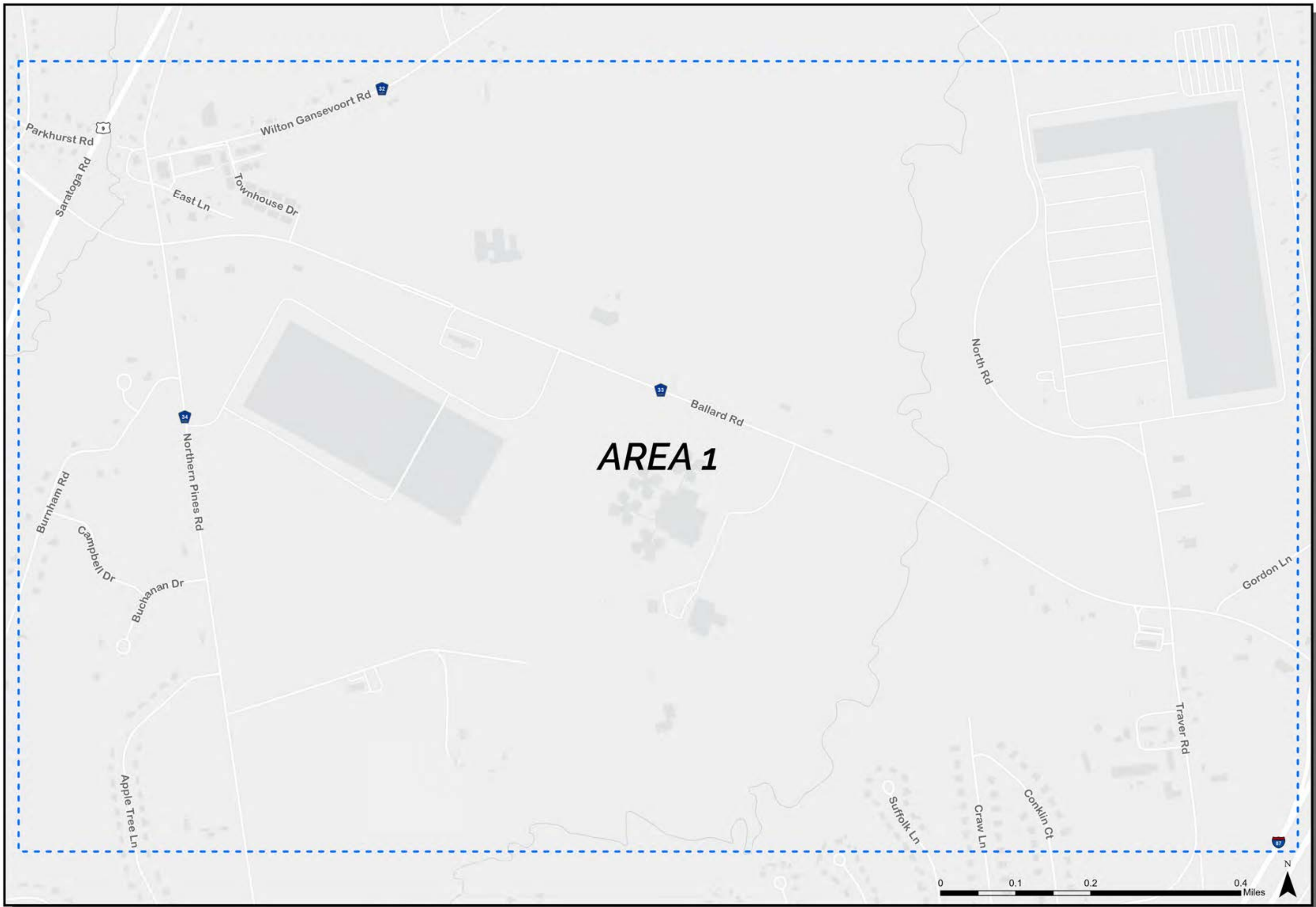
**LEGEND**

- Wilton Town Boundary
- Survey Locations

**SIDEWALK ASSESSMENT**

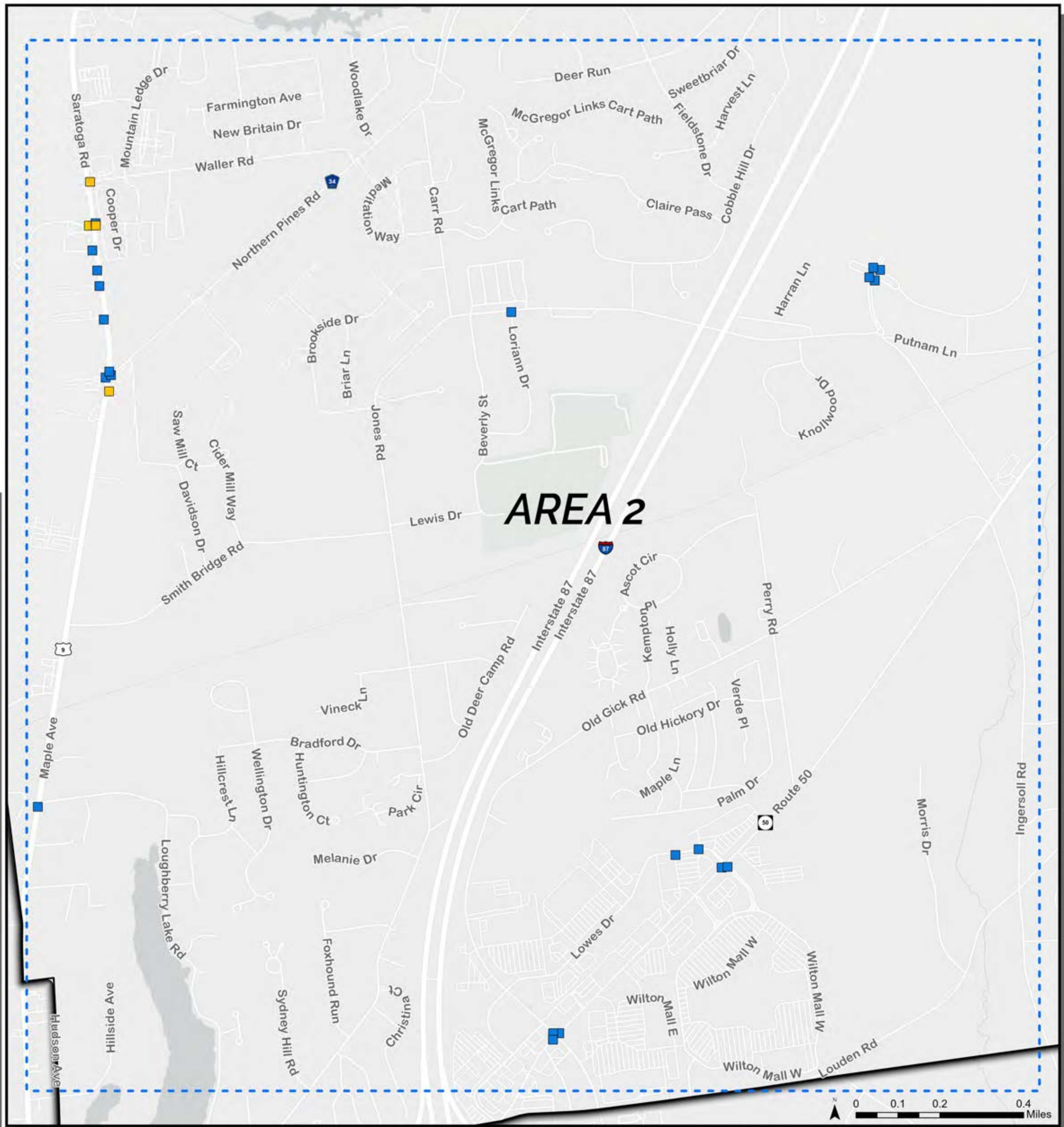
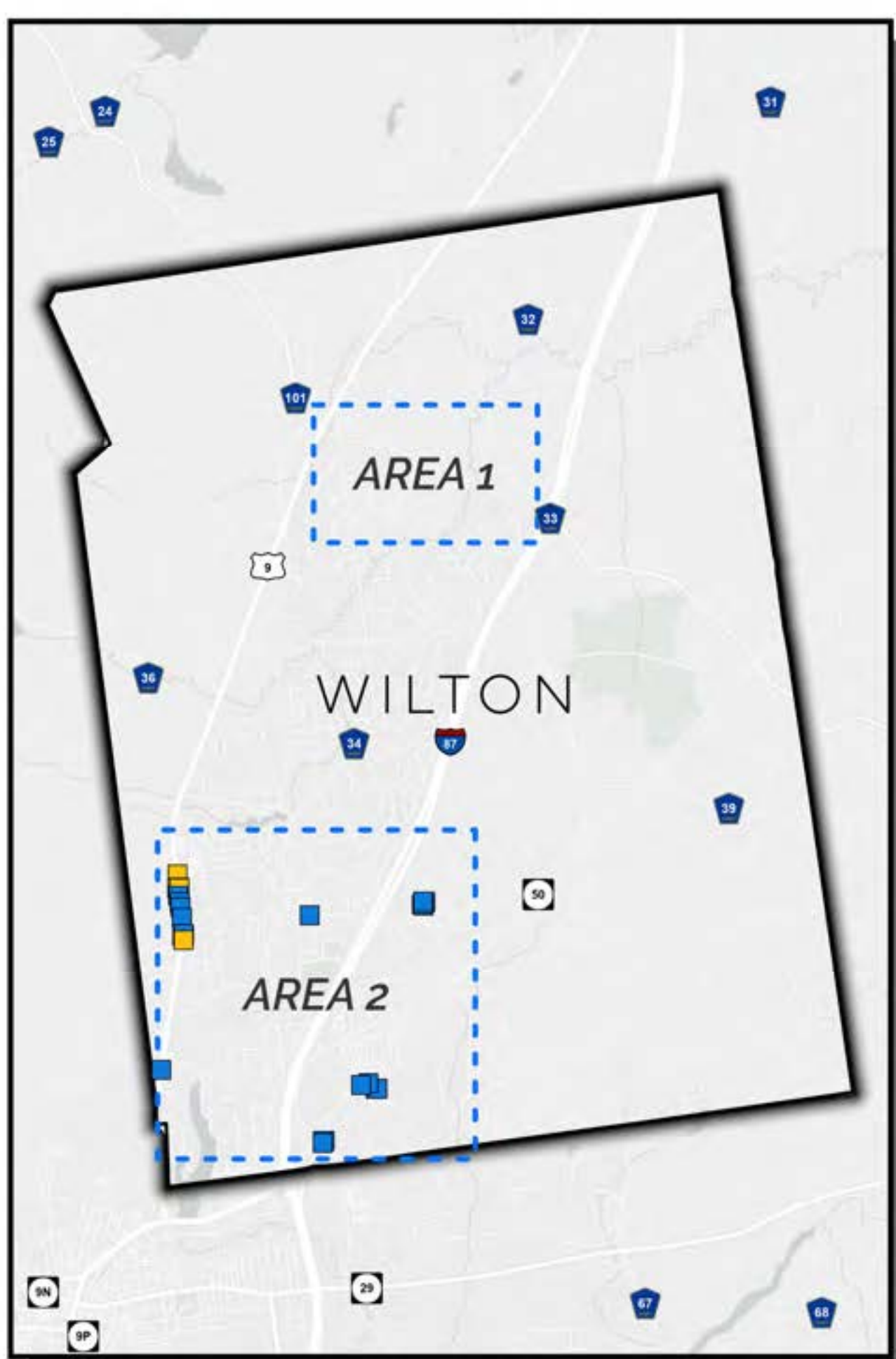
- Fully Accessible
- Mostly Accessible
- Partially Accessible
- Not Accessible

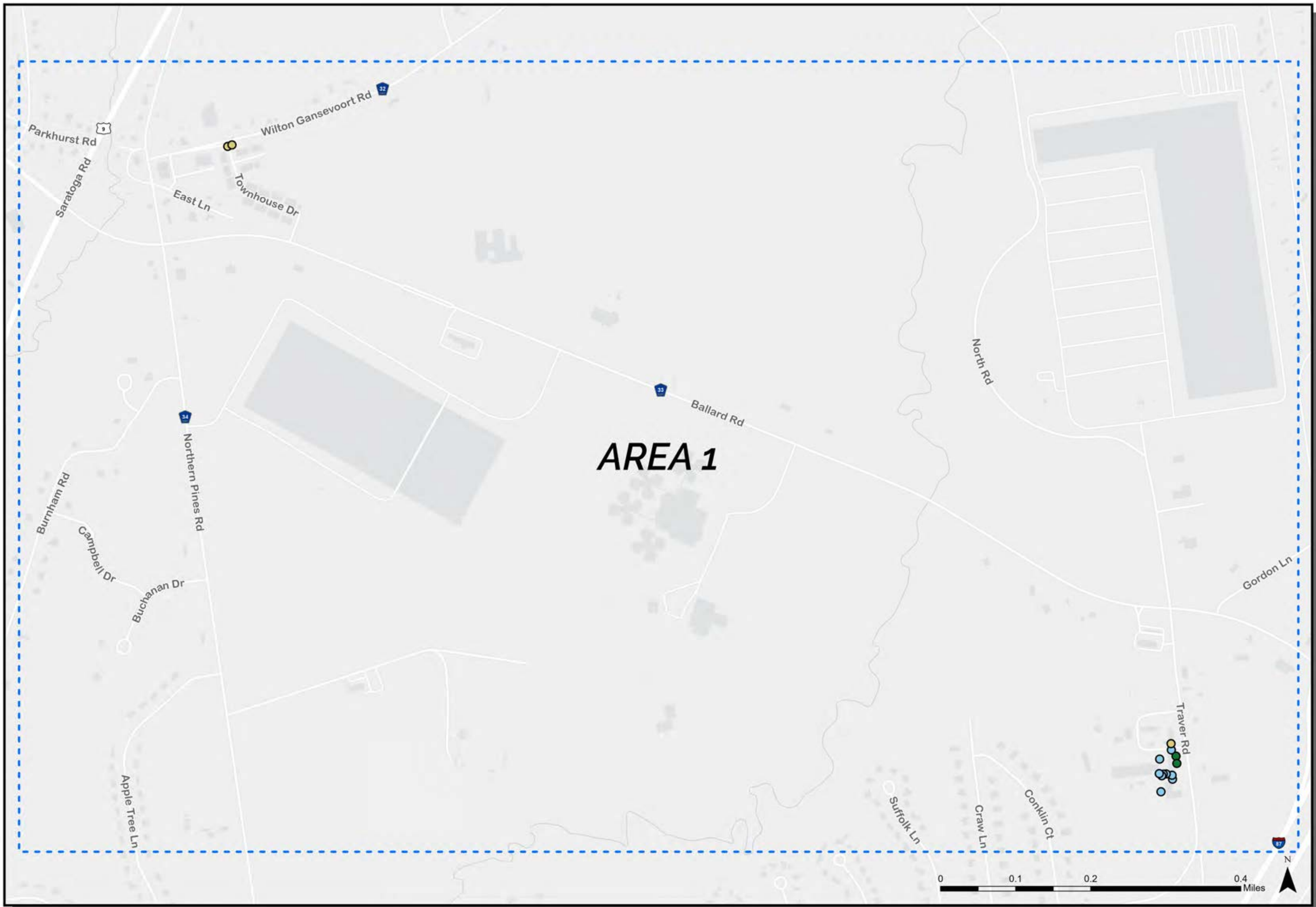




**LEGEND**

- Wilton Town Boundary
- Survey Locations
- CROSSWALK ASSESSMENT**
- Accessible
- Inaccessible



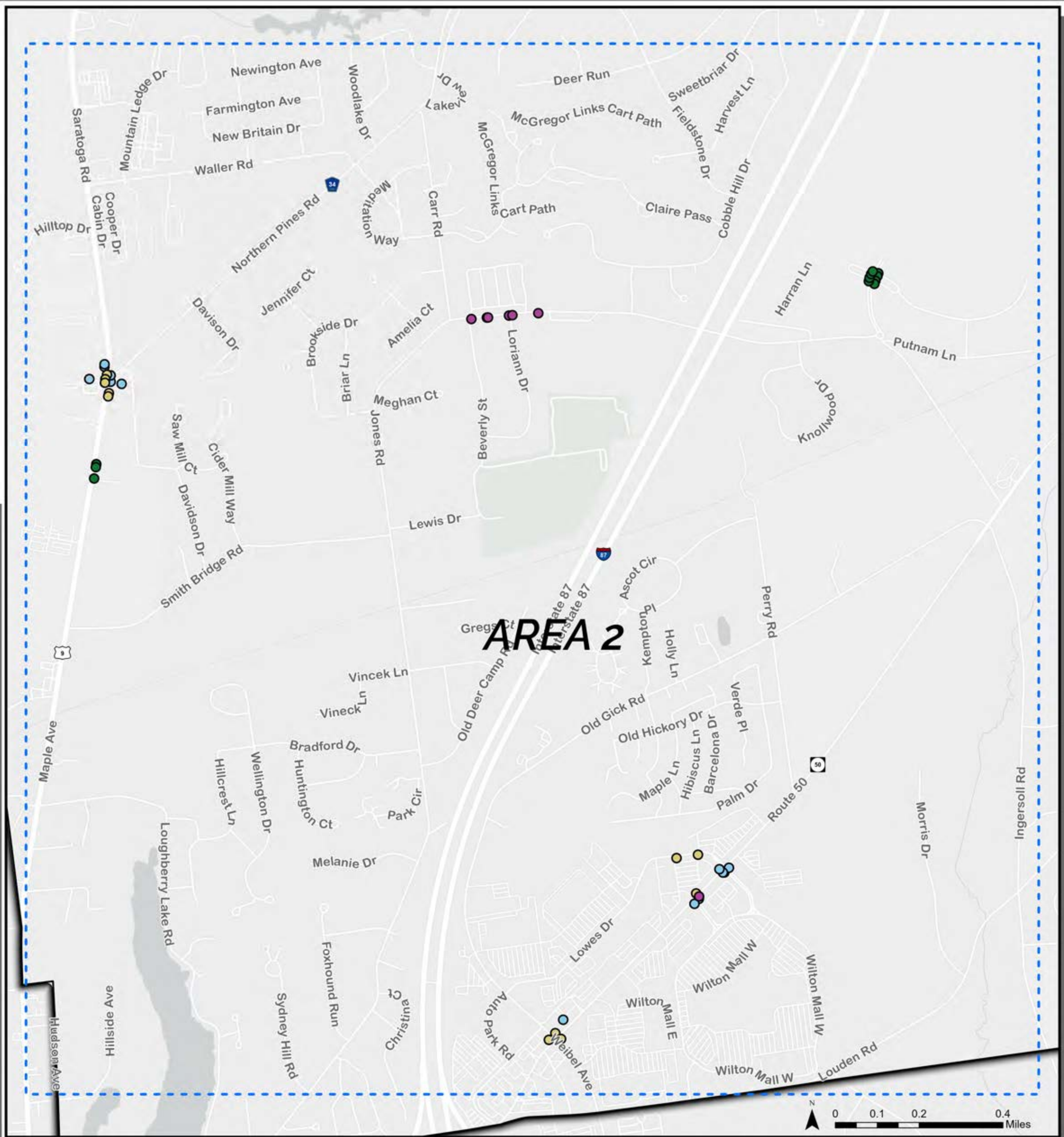


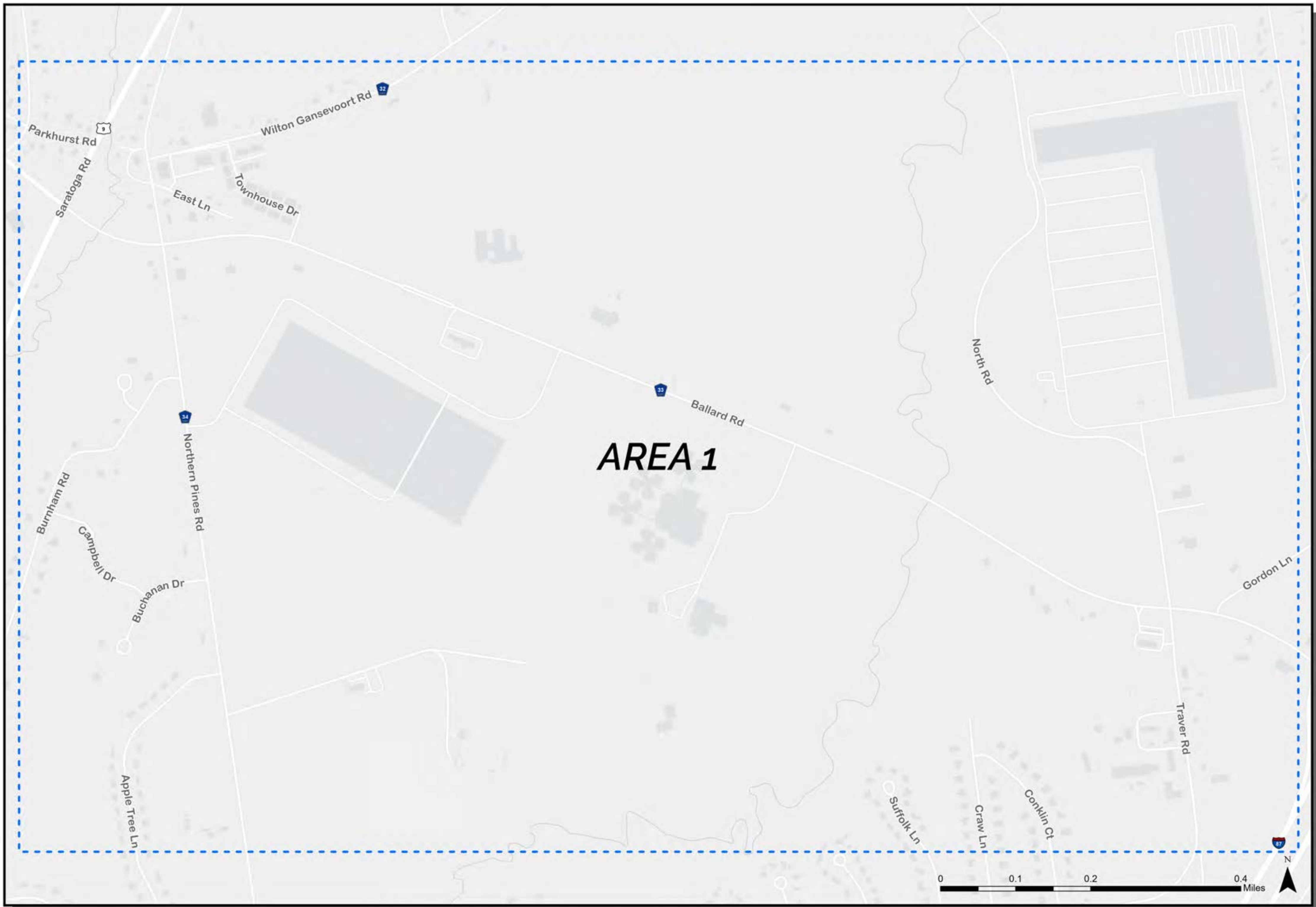
**LEGEND**

- Wilton Town Boundary
- Survey Locations

**CURB RAMP ASSESSMENT**

- Fully Accessible
- Mostly Accessible
- Partially Accessible
- Not Accessible



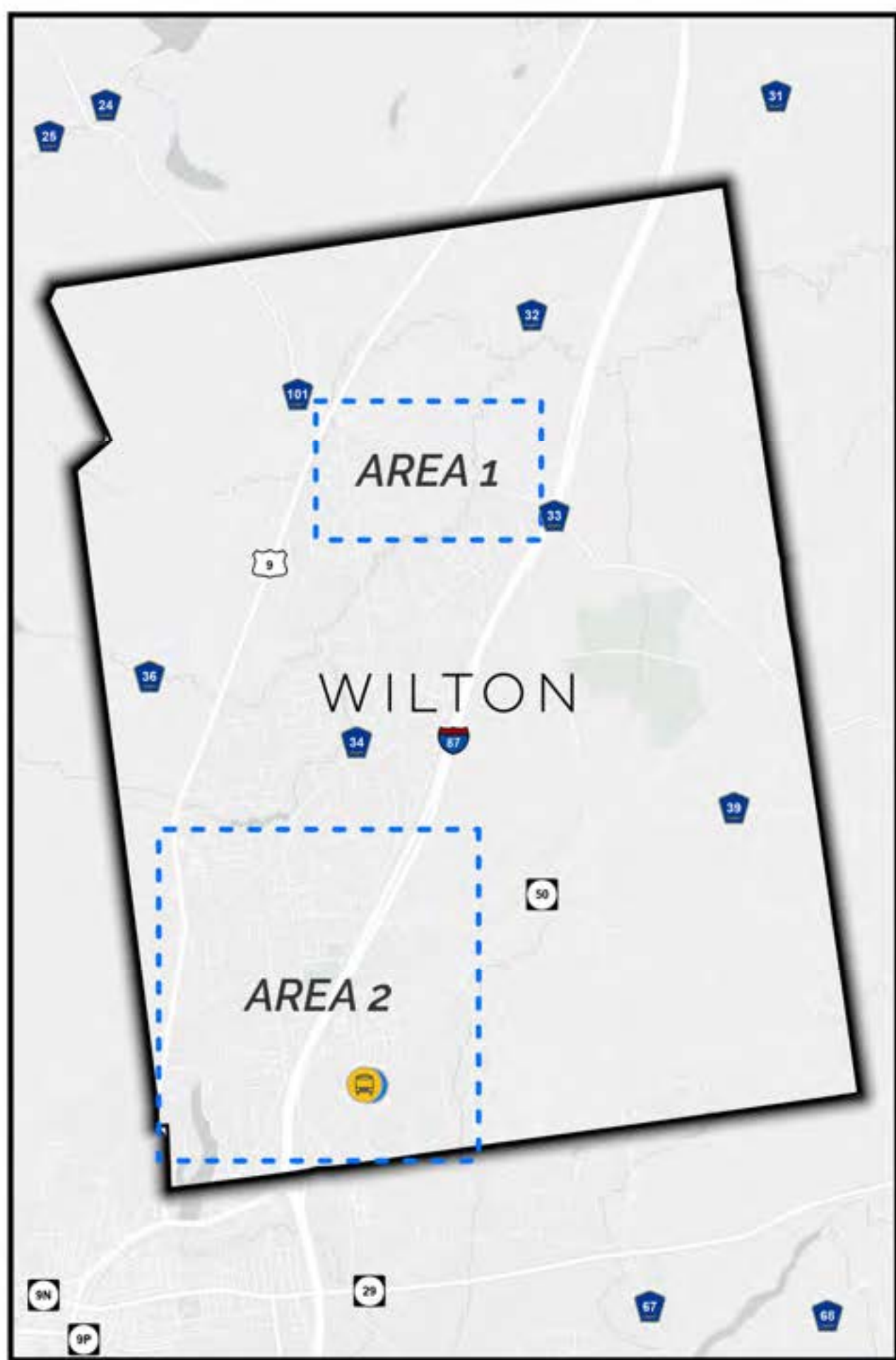


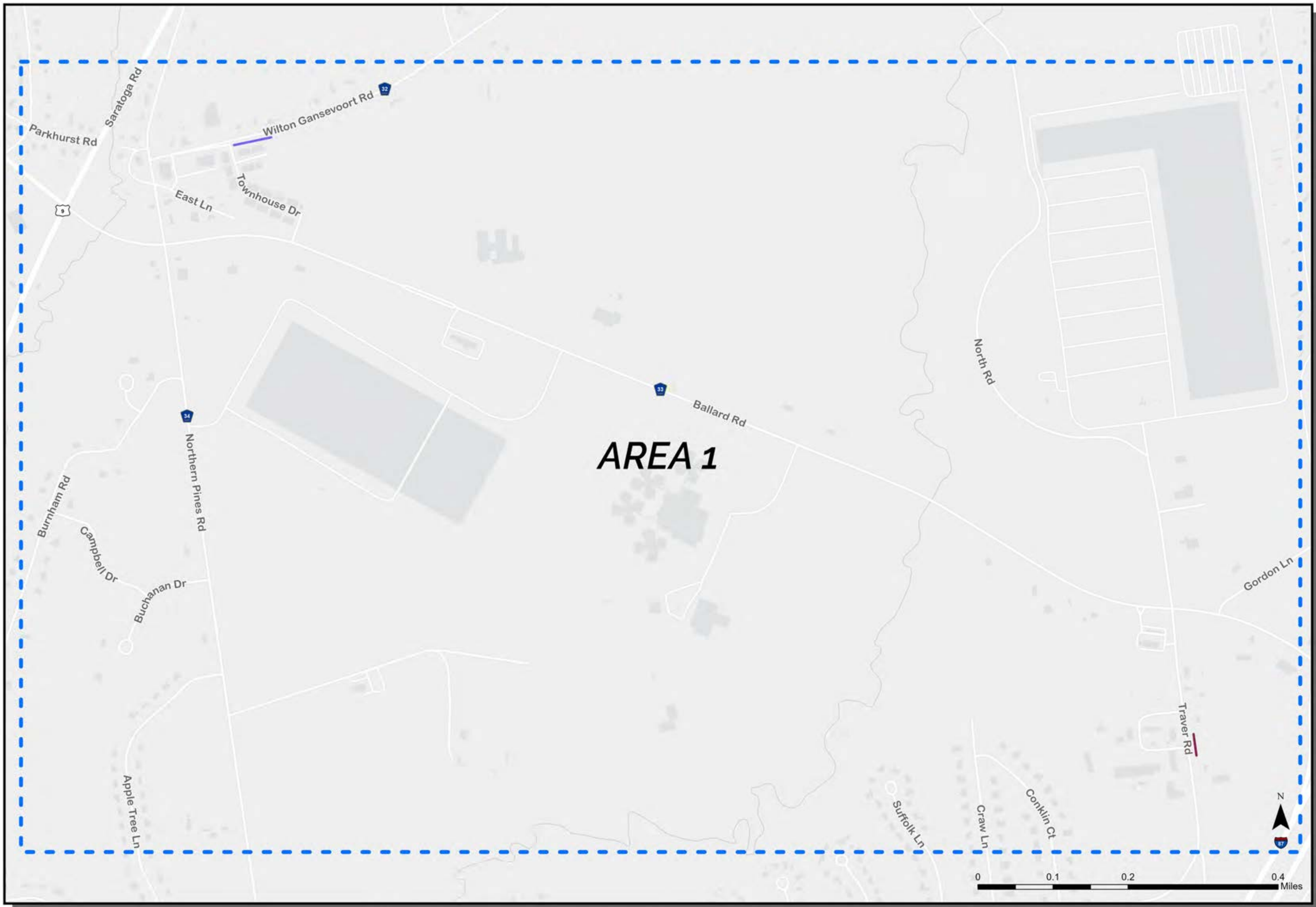
**LEGEND**

- Wilton Town Boundary
- ▭ Survey Locations

**TRANSIT STOPS**

- Accessible
- Inaccessible



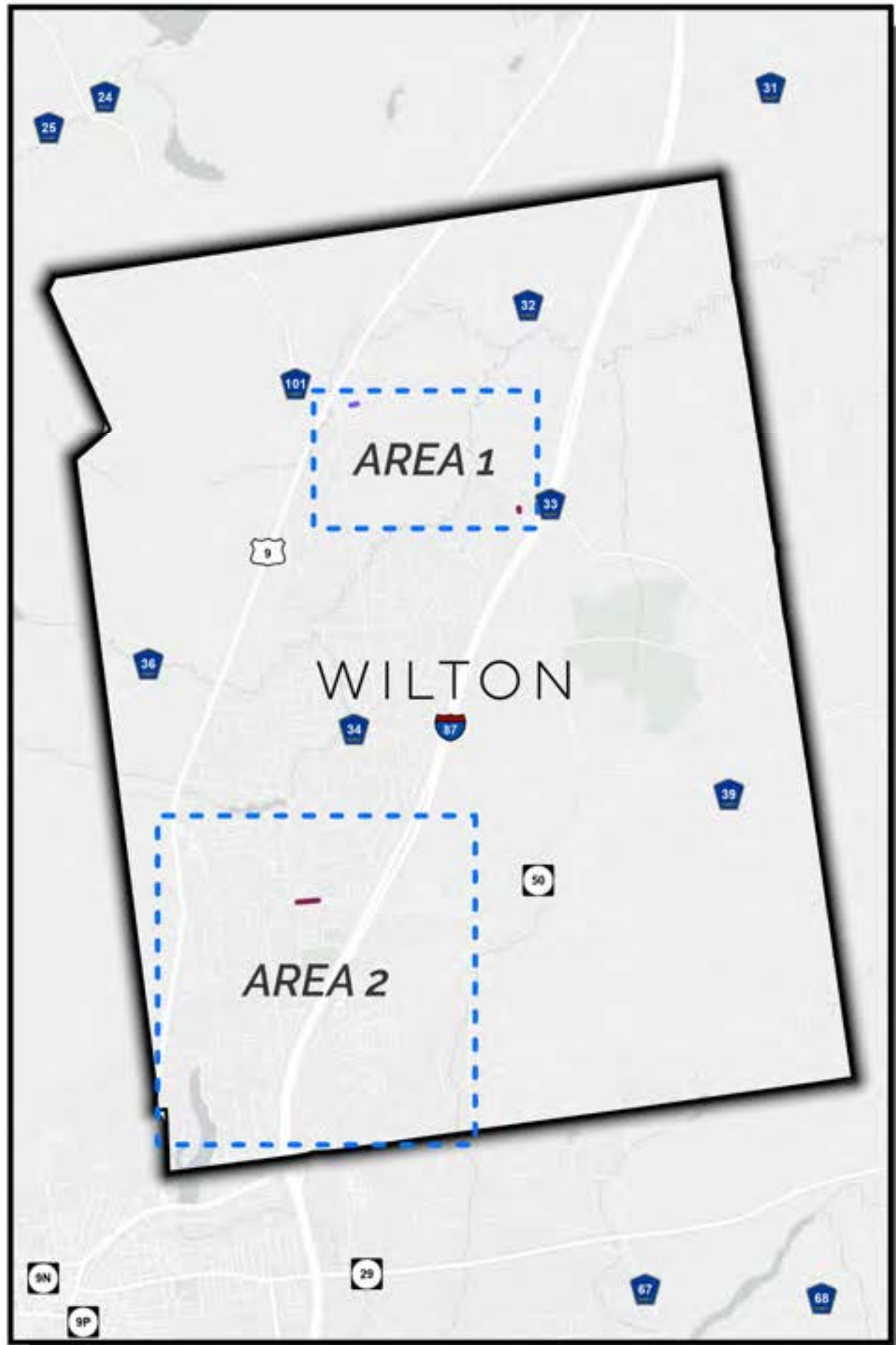


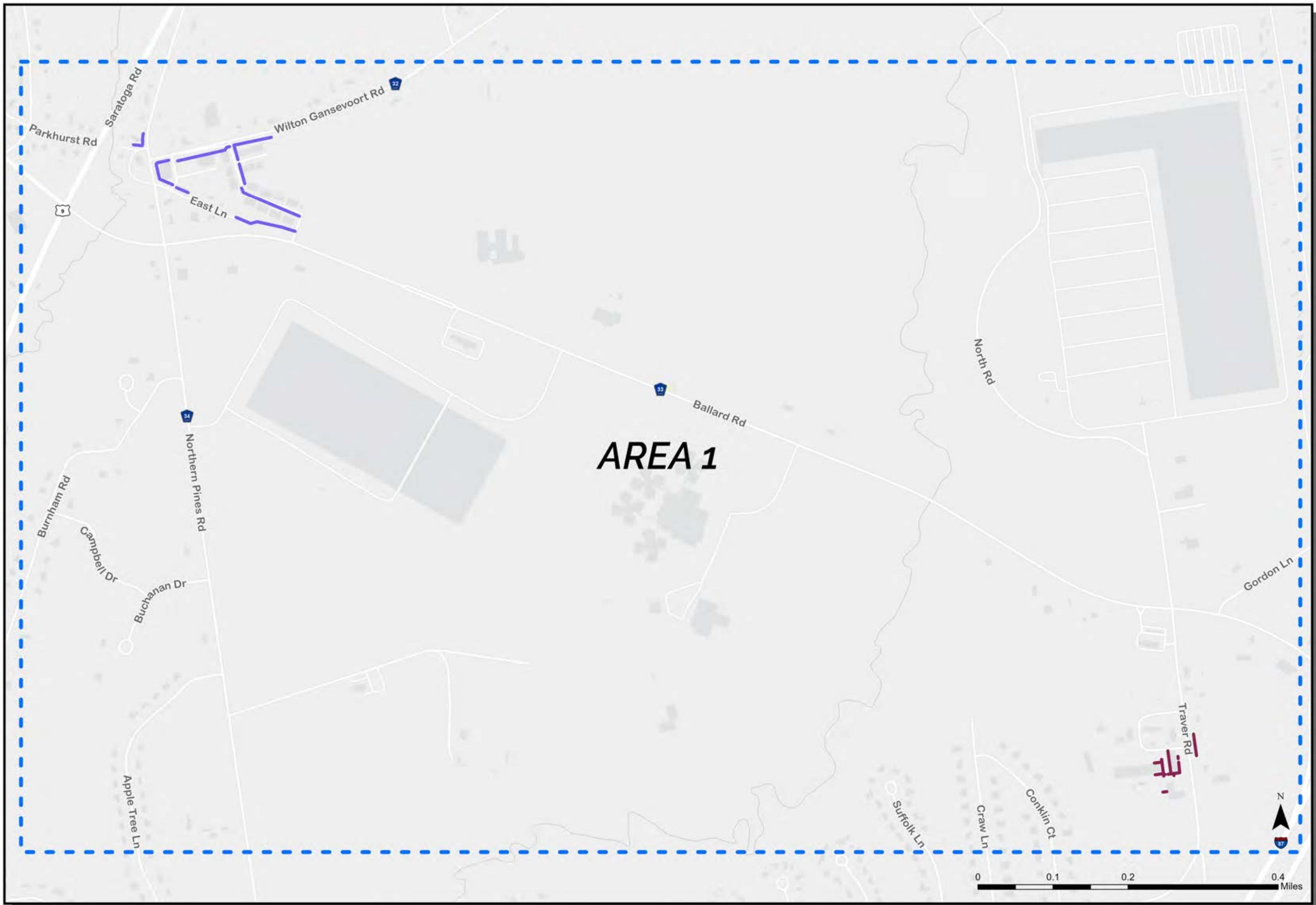
**LEGEND**

- Wilton Town Boundary
- Survey Locations

**SIDEWALK PRIORITIZATION**  
(PARTIALLY ACCESSIBLE & INACCESSIBLE FEATURES)

- Priority 1 - Government Facilities, Parks, and Recreation areas
- Priority 2 - Hamlet Zones
- Priority 3 - Dense Retail
- Priority 4 - Residential Areas
- Priority 5 - Others



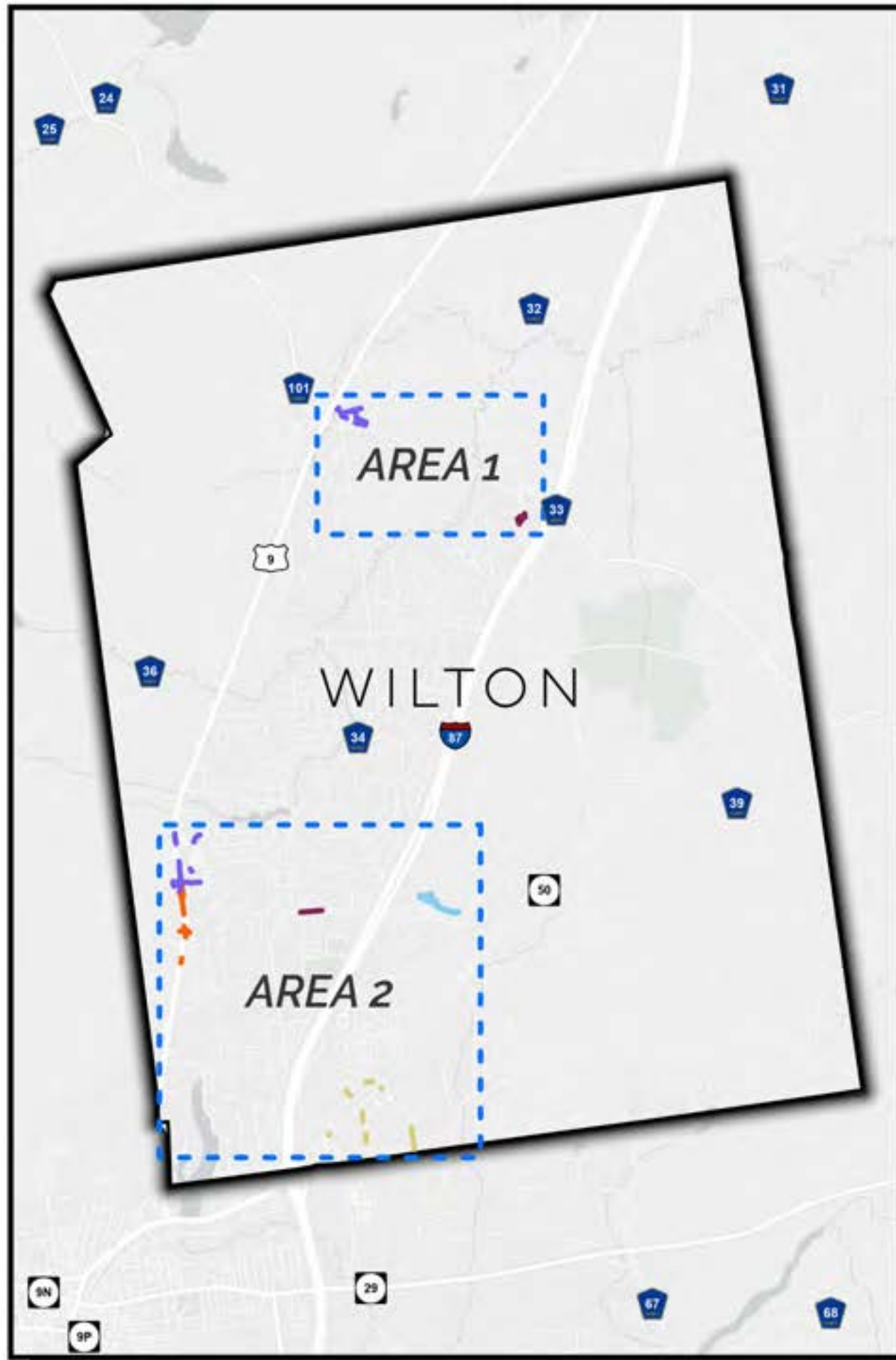


**LEGEND**

- Wilton Town Boundary
- Survey Locations

**SIDEWALK PRIORITIZATION (FULL INVENTORY)**

- Priority 1 - Government Facilities, Parks, and Recreation areas
- Priority 2 - Hamlet Zones
- Priority 3 - Dense Retail
- Priority 4 - Residential Areas
- Priority 5 - Others



## Appendix C: Cost Estimates

Unit cost estimates are established using replacement costs from the NYSDOT Quick Estimator Reference – Upstate. Unit cost estimates are used in conjunction with field data to estimate repair costs. Note that specific costs may vary due to site conditions that may require relocating utilities/mailboxes; alternating drainage structures and driveway aprons; acquiring right-of-way, pruning/clearing & grubbing, and planting. These cost estimates are not adjusted for inflation. Typical sidewalk replacement costs (2023) are observed at \$13 to \$15 per square foot, or \$65 to \$75 per linear foot (LF). For the purpose of budgeting in this report \$75 is recommended to assess cost. Additionally, based on jurisdiction outlined in Table 1 of this report, Responsible Entity for Maintenance of Pedestrian Facilities, costs of repairs for curb ramps, pedestrian signals, and crosswalks below only include right of ways under the jurisdiction and responsibility of the Town.

**Table 1 Unit Cost Estimates**

Facility	Cost
Concrete Sidewalk (4" thick, 5' wide)	\$75/linear foot
DWS	\$2,000
ADA Curb Ramp	\$6,300
LS Type Ladder Crosswalk (700ft Typical)	\$1,400
Pedestrian Signal	\$8,200

**Table 2 Town of Wilton ADA Compliance Estimated Costs**

Sidewalks Estimated Cost for Compliance:	
Partially Accessible (528ft)*	\$5,940.00
Not Accessible (792ft)**	\$17,820.00
<b>Total Non-ADA Compliant Segments ( )</b>	<b>\$23,760.00</b>
<i>*Assumed 15% of panels in need of replacement</i>	
<i>**Assumed 30% of panels in need of replacement</i>	
Curb Ramps and Signals Estimated Costs for Compliance:	
14 Curb Ramps*	\$88,200.00
48 DWS	\$96,000.00
0 Pedestrian Signals	\$0.00
<b>Total Intersections:</b>	<b>\$184,200.00</b>
<i>*Excludes "Not Flush with Asphalt"</i>	
Crosswalks Estimated Costs for Compliance:	

<b>Total Crosswalks*</b>	<b>\$0.00</b>
<i>*Assumed incomplete access route is addressed in curb cut replacements</i>	
<b>Total Capital Investment</b>	<b>\$ 207,960.00</b>

## **Appendix D: Work Plan and Data Inventory**

The Town of Wilton is committed to ensuring facilities in the public right of way are accessible for everyone, including people with disabilities. The Town will prioritize:

**Priority 1:** Facilities serving government facilities, parks and recreation areas.

**Priority 2:** Facilities serving hamlet zones;

**Priority 3:** Facilities serving dense retail centers;

**Priority 4:** Facilities serving residential areas; and

**Priority 5:** Facilities serving other areas

The facilities that are prioritized first are most likely to be frequented by residents and visitors and are essential for the Town's continued operation and success. A map of all sidewalk segments by priority ranking is in Appendix B.

The "Sidewalk Segments in Priority Areas" table within this appendix will be used to identify sidewalk segments eligible for repair/replacement based on their priority ranking. Sidewalk segments will then be cross referenced with field analysis results to determine specific barriers to accessibility and the repair/replacement needs for all facilities associated with the segment (a guide to using the field analysis results is included within this appendix). The Town will then attempt to work with adjacent landowners to remove barriers to accessibility until each segment is brought to compliance, beginning with the highest priority (1), and ending with the last priority (5). For example, sidewalk segments ranked in priority category 1 will be first to be repaired/replaced, followed by segments ranked in category 2, and so on. The same process will be used to bring intersections, and crossings, to compliance using the corresponding tables within this appendix.

The Town may deviate from this framework to take advantage of regular capital improvement projects or private development to upgrade deficient facilities, if occurring adjacent to those facilities. For example: a low priority that is rated "Not Accessible" may be brought to compliance in conjunction with an adjacent road resurfacing project before the Town has finished bringing higher priority segments to compliance.

As previously discussed under the Jurisdiction section of this report, if another jurisdiction is undertaking alterations of a roadway, that agency is responsible for the replacement of removed pedestrian facilities or for the construction or rehabilitation of a curb ramp to the adjacent sidewalk if one does not exist or has become inaccessible. The NYS Department of Transportation (NYSDOT) has completed a Transition Plan including sidewalks along state roadways and undertakes on-going road maintenance and alterations. For the purposes of this Transition Plan, areas lacking the provision of curb ramps in state rights-of-way will be identified in this plan as being undertaken by NYSDOT. Pedestrian signals and crosswalks on state roadways will also be identified as being undertaken by NYSDOT. The improvement of these facilities will correspond to the approved NYSDOT capital improvement program.

The table below is intended to be used as an identification tool for existing barriers and is therefore inclusive of all sidewalk segments in need of improvement, regardless of jurisdiction. For those sidewalk sections falling along County or State Roads, the Town will coordinate with NYSDOT or the responsible entity to address repairs.

# SIDEWALKS

Accessibility Rating	Sidewalk Address & Jurisdiction	Side of Street	Priority	Are there any defects?	Defects observed
Not Accessible	Jones Rd - <b>Town</b>	South	Priority 4	Yes	Cracking, Too_narrow
Not Accessible	Jones Rd - <b>Town</b>	South	Priority 4	Yes	Cracking, Too_narrow
Partially Accessible	Traver Rd - <b>Private</b>	East	Priority 1	Yes	Other
Partially Accessible	Wilton Gansevoort Rd - <b>Private</b>	South	Priority 5	Yes	Cracking
Mostly Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Mostly Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Mostly Accessible	Saratoga Rd (U.S. Route 9)	East	Priority 2	Yes	Obstructions
Mostly Accessible	Market 32 by Price Chopper <b>Private</b>		Priority 3	No	
Mostly Accessible	Wilton Mall <b>County</b>		Priority 3	No	
Mostly Accessible	Walgreen's <b>Private</b>		Priority 3	No	
Mostly Accessible	East La <b>Private</b>	North	Priority 5	Yes	Cracking
Mostly Accessible	East La <b>Private</b>	North	Priority 5	Yes	Cracking
Mostly Accessible	Northern Pines Rd <b>Private</b>	East	Priority 5	No	
Mostly Accessible	Parkhurst Rd <b>Private</b>	North	Priority 5	No	
Mostly Accessible	Northern Pines Rd <b>Private</b>	West	Priority 5	No	
Mostly Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 5	No	
Mostly Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 5	No	
Mostly Accessible	Wilton Commons Senior Apts		Priority 5	No	
Mostly Accessible	Heritage Way <b>Private</b>	North	Priority 5	No	
Mostly Accessible	Heritage Way <b>Private</b>	North	Priority 5	No	
Mostly Accessible	Heritage Way <b>Private</b>	North	Priority 5	No	
Mostly Accessible	Heritage Way <b>Private</b>	North	Priority 5	No	
Mostly Accessible	Hilltop Dr <b>Town</b>	North	Priority 5	No	
Mostly Accessible	Hilltop Dr <b>Town</b>	South	Priority 5	No	
Mostly Accessible	Renaissance La <b>Private</b>	East	Priority 5	No	
Mostly Accessible	Mountain Ledge Dr <b>Private</b>	West	Priority 5	No	
Mostly Accessible	Ridgeview Commons <b>Private</b>		Priority 5	Yes	Cracking
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Are there any defects?	Defects observed
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Town of Wilton Municipal Complex		Priority 1	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	West	Priority 2	No	
Fully Accessible	Maple Ave (U.S. Route 9)	East	Priority 2	No	
Fully Accessible	U.S. Route 9	East	Priority 2	Yes	Cracking
Fully Accessible	U.S. Route 9	East	Priority 2	No	
Fully Accessible	U.S. Route 9	East	Priority 2	No	
Fully Accessible	U.S. Route 9	West	Priority 2	No	
Fully Accessible	CVS parking lot - <b>Private</b>		Priority 2	No	
Fully Accessible	Marketplace Rd - <b>Town</b>	East	Priority 3	No	
Fully Accessible	Spectrum at Wilton Marketplace - <b>Private</b>		Priority 3	No	
Fully Accessible	Wendy's <b>Private</b>		Priority 3	No	
Fully Accessible	Lowes Dr <b>Private</b>	South	Priority 3	No	
Fully Accessible	Ocala Ct <b>Town</b>	West	Priority 4	No	
Fully Accessible	Branch Blvd <b>Town</b>	West	Priority 4	No	
Fully Accessible	Branch Blvd <b>Town</b>	East	Priority 4	No	
Fully Accessible	Daintree Dr <b>Town</b>	North	Priority 4	No	
Fully Accessible	Daintree Dr <b>Town</b>	South	Priority 4	No	
Fully Accessible	Branch Blvd <b>Town</b>	West	Priority 4	No	
Fully Accessible	Branch Blvd <b>Town</b>	East	Priority 4	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	East	Priority 5	No	

Accessibility Rating	Sidewalk Address	Side of Street	Priority	Are there any defects?	Defects observed
Fully Accessible	Saratoga Rd (U.S. Route 9)	East	Priority 5	No	
Fully Accessible	Saratoga Rd (U.S. Route 9)	East	Priority 5	No	

# CURB RAMPS

Overall Intersection Rating	Road Name	Street Side	Priority	Curb Ramp Present?	Curb Ramp Flush?	Sufficient Curb Ramp Width?	DWS Present?	Sufficient DWS Width?	Sufficient DWS Depth?	Sufficient DWS Color?	Sufficient DWS Condition?
Not Accessible	U.S. Route 50	South	Priority 3	Yes	Yes	No	No				
Not Accessible	Jones Rd	South	Priority 4	No			No				
Not Accessible	Jones Rd	South	Priority 4	No			No				
Not Accessible	Jones Rd	South	Priority 4	No			No				
Not Accessible	Loriann Dr	West	Priority 4	No			No				
Not Accessible	Loriann Dr	East	Priority 4	No			No				
Not Accessible	Jones Rd	South	Priority 4	No			No				
Partially Accessible	Town of Wilton Municipal Complex		Priority 1	Yes	Yes	No	No				
Partially Accessible	U.S. Route 9	West	Priority 2	No			No				
Partially Accessible	U.S. Route 9	West	Priority 2	Yes	Yes	Yes	Yes	Yes	Yes	No	No
Partially Accessible	U.S. Route 9	West	Priority 2	Yes	Yes	Yes	Yes	Yes	Yes	No	No
Partially Accessible	U.S. Route 9	East	Priority 2	No			No				
Partially Accessible	U.S. Route 9	East	Priority 2	No			No				
Partially Accessible	U.S. Route 50	North	Priority 3	Yes	Yes	Yes	No				
Partially Accessible	U.S. Route 50	North	Priority 3	Yes	Yes	Yes	No				
Partially Accessible	U.S. Route 50	North	Priority 3	Yes	Yes	Yes	No				
Partially Accessible	U.S. Route 50	South	Priority 3	Yes	Yes	No	No				









# CROSSWALKS

ADA Accessibility Rating?	Road Name	Priority	Is there a pedestrian access route?	Is the route smooth, level, and free of barriers?	Painted/designated crosswalk?	Condition of the crosswalk paint/material?
Inaccessible	Adirondack Trust Driveway	Priority 2	Yes	Yes	Yes	Fair
Inaccessible	Waller Rd	Priority 5	Yes	Yes	No	
Inaccessible	Hilltop Dr	Priority 5	Yes	Yes	Yes	Poor
Inaccessible	Gesing Blvd	Priority 5	Yes	Yes	Yes	Poor
Accessible	Renaissance Ln	Priority 2	Yes	Yes	No	
Accessible	Driveway	Priority 2	Yes	Yes	No	
Accessible	Park Place at Wilton Driveway	Priority 2	Yes	Yes	No	
Accessible	Park Place at Wilton Driveway	Priority 2	Yes	Yes	No	
Accessible	CVS Driveway	Priority 2	Yes	Yes	Yes	Good
Accessible	Northern Pines Rd	Priority 2	Yes	Yes	Yes	Good
Accessible	U.S. Route 9	Priority 2	Yes	Yes	Yes	Good
Accessible	Route 50	Priority 3	Yes	Yes	Yes	Good
Accessible	Wilton Mall W	Priority 3	Yes	Yes	Yes	Good
Accessible	Lowes Dr	Priority 3	Yes	Yes	Yes	Good
Accessible	Route 50	Priority 3	Yes	Yes	Yes	Good
Accessible	Old Gick Rd	Priority 3	Yes	Yes	Yes	Good
Accessible	Route 50	Priority 3	Yes	Yes	Yes	Good
Accessible	Eastern Mountain Sports Entrance	Priority 3	Yes	Yes	Yes	Good
Accessible	Loriann Dr	Priority 4	Yes	Yes	Yes	Good
Accessible	Daintree Dr	Priority 4	Yes	Yes	No	

Accessible	Daintree Dr	Priority 4	Yes	Yes	No	
Accessible	Branch Blvd	Priority 4	Yes	Yes	No	
Accessible	Ocala Ct	Priority 4	Yes	Yes	No	
Accessible	Ocala Ct	Priority 4	Yes	Yes	No	
Accessible	Gesing Blvd	Priority 5	Yes	Yes	No	
Accessible	U.S. Route 9	Priority 5	Yes	Yes	Yes	Good



## Appendix E: ADA Policy Statement

### NOTICE UNDER THE AMERICANS WITH DISABILITIES ACT

In accordance with the requirements of title II of the Americans with Disabilities Act of 1990 ("ADA"), the Town of Wilton will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities.

**Employment:** The Town of Wilton does not discriminate on the basis of disability in its hiring or employment practices and complies with all regulations promulgated by the U.S. Equal Employment Opportunity Commission under title I of the ADA.

**Effective Communication:** The Town of Wilton will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities so they can participate equally in Town programs, services, and activities, including qualified sign language interpreters, documents in Braille, and other ways of making information and communications accessible to people who have speech, hearing, or vision impairments.

**Modifications to Policies and Procedures:** The Town of Wilton will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities. For example, individuals with service animals are welcomed in Town offices, even where pets are generally prohibited.

Anyone who requires an auxiliary aid or service for effective communication, or a modification of policies or procedures to participate in a program, service, or activity of the Town of Wilton, should contact the office of the ADA Coordinator as soon as possible but no later than 48 hours before the scheduled event.

The ADA does not require the Town of Wilton to take any action that would fundamentally alter the nature of its programs or services or impose an undue financial or administrative burden.

Complaints that a program, service, or activity of the Town of Wilton is not accessible to persons with disabilities should be directed to the ADA Coordinator, the Secretary to the Supervisor, [info@townofwilton.com](mailto:info@townofwilton.com).

The Town of Wilton will not place a surcharge on a particular individual with a disability or any group of individuals with disabilities to cover the cost of providing auxiliary aids/services or reasonable modifications of policy, such as retrieving items from locations that are open to the public but are not accessible to persons who use wheelchairs.

## **Appendix F: Grievance Procedure**

### **Town of Wilton**

#### **Grievance Procedure under the Americans with Disabilities Act**

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 ("ADA"). It may be used by anyone who wishes to file a complaint alleging discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the Town of Wilton. The Town's Personnel Policy governs employment-related complaints of disability discrimination.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint, will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or his/her designee as soon as possible but no later than 60 calendar days after the alleged violation to:

**Secretary to the Supervisor  
ADA Coordinator  
22 Traver Rd  
Wilton, NY 12831**

Within 15 calendar days after receipt of the complaint, the Secretary to the Supervisor, or their designee will meet with the complainant to discuss the complaint and the possible resolutions. Within 15 calendar days of the meeting, the ADA Coordinator or their designee will respond in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The response will explain the position of the Town of Wilton and offer options for substantive resolution of the complaint.

If the response by the ADA Coordinator or their designee does not satisfactorily resolve the issue, the complainant and/or his/her designee may appeal the decision within 15 calendar days after receipt of the response to the Town Supervisor or his designee.

Within 15 calendar days after receipt of the appeal, the Town Supervisor or his designee will meet with the complainant to discuss the complaint and possible resolutions. Within 15 calendar days after the meeting, the Town Supervisor or his designee will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by the ADA Coordinator or their designee, appeals to the Town Supervisor or his designee, and responses from these two offices will be retained by the Town for at least three years.

## **Appendix G: ADA Coordinator Contact Information**

### **Town of Wilton ADA Coordinator**

All complaints and/or questions regarding accessibility of any public buildings or pedestrian facilities within the right-of-way of the Town of Wilton should be directed to designated official below.

**Title: Secretary to the Supervisor**

**Telephone: (518) 587-1939**

**Fax Number: (518) 587-2837**

**Email: [info@townofwilton.com](mailto:info@townofwilton.com)**

**Mailing Address: 22 Traver Road, Wilton, NY 12831**

## **Appendix H: Public Comment**

**Appendix I: Town Code - Construction and Maintenance of Sidewalks**

**§ 107-4. Construction and maintenance of sidewalks.**

- A. (Reserved)<sup>1</sup>
- B. The Town Board may construct, repair or maintain, or may order the construction, repair and maintenance of sidewalks for the health, safety and general welfare of the residents of the Town after notifying the involved property owners of the time and place of a hearing of such order before the Town Board. No work shall be commenced until approved by either the Town Highway Superintendent or State Department of Transportation or Saratoga County having jurisdiction over the right-of-way/easement within which the sidewalk is located, if necessary.
- C. (Reserved)<sup>2</sup>
- D. The owner or owners of all lots, parcels and premises within the Town are required to maintain, repair and keep safe sidewalks adjacent to or upon their property and premises in or along the public street in the Town.
- E. It shall be the duty of all owners of premises within the limits of the Town to keep all sidewalks which have been heretofore or hereafter laid in front of, upon, or adjacent to such premises, in or along any of the street right-of-way, in good repair and free from dangerous ice, snow, or other dangerous obstructions and conditions. Any owner of any such premises who shall allow any such sidewalk to remain in disrepair or in a dangerous condition shall be responsible and liable for injuries and damages arising out of the disrepair or unsafe condition of said sidewalks. Such owner shall further indemnify and reimburse the Town for any and all liability, costs and expenses, which the Town might incur as a result of any such defective or dangerous sidewalks.
- F. Whenever any snow or ice shall fall or drift on or across any sidewalk, the owner or occupant of the lot, building or other premises adjacent to or abutting the sidewalk shall remove such snow and ice or cause the same to be removed within a reasonable of time, as determined by the Town.
- G. No person shall permit or cause any building material, dirt, sand, excavated material, wood, rubbish, any article or other substance or merchandise to be dropped, delivered, piled or placed in any way above or upon any sidewalk or pathway so as to obstruct the sidewalk or pathway except by special permission of the Town. Merchandise necessarily delivered on the sidewalks or pathways shall be immediately removed to the interior of the address to which it was destined.

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1. **Editor's Note: Former Subsection A, regarding Town Board reservation of the right to construct sidewalks in the H-1 and CR-2 Districts, as amended, was repealed 2-7-2013.**

2. **Editor's Note: Former Subsection C, concerning sidewalk area being considered part of the green space requirement, was repealed 2-7-2013.**